



**Heritage  
Advisory Committee  
Meeting Agenda**

**Monday, March 5, 2018  
7 p.m.**

**Holland Room  
Aurora Town Hall**

Public Release  
February 26, 2018



## **Town of Aurora Heritage Advisory Committee Meeting Agenda**

**Date:** Monday, March 5, 2018

**Time and Location:** 7 p.m., Holland Room, Aurora Town Hall

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### **1. Approval of the Agenda**

**Recommended:**

That the agenda as circulated by Legislative Services be approved.

### **2. Declarations of Pecuniary Interest and General Nature Thereof**

### **3. Receipt of the Minutes**

**Heritage Advisory Committee Meeting Minutes of February 12, 2018**

**Recommended:**

That the Heritage Advisory Committee meeting minutes of February 12, 2018, be received for information.

### **4. Delegations**

### **5. Matters for Consideration**

- 1. HAC18-004 – Request to Designate under Part IV of the *Ontario Heritage Act*, 19 and 21 Machell Avenue “The John van Nostrand House”**

**Recommended:**

1. That Report No. HAC18-004 be received; and
2. That the Heritage Advisory Committee recommend to Council:
  - (a) That the House located at 19 and 21 Machell Avenue be designated under Part IV of the *Ontario Heritage Act* as a property of cultural heritage value or interest; and
  - (b) That the Town Clerk be authorized to publish and serve Council's Notice of Intention to Designate as per requirements of the Act; and
  - (c) That the designation by-law be brought before Council for passing if no objections were received within the thirty (30) day objection period as per requirements of the Act; and
  - (d) That the owners of 19 and 21 Machell Avenue be thanked for their support of the designation of the subject heritage property.

**2. HAC18-005 – Additional Information: Request to Remove a Property from the Aurora Register of Properties of Cultural Heritage Value or Interest  
14452 Yonge Street**

**Recommended:**

1. That Report No. HAC18-005 be received; and
2. That the Heritage Advisory Committee recommend to Council:
  - (a) That 14452 Yonge Street be removed from the Aurora Register of Properties of Cultural Heritage Value or Interest; and
  - (b) That a financial contribution of \$75,000.00 be provided to the Town's Heritage Reserve Fund.

**3. HAC18-006 – East Holland River, Fish Barrier Removal, Restoration and Bridge Replacement**

**Recommended:**

1. That Report No. HAC18-006 be received; and
2. That the Heritage Advisory Committee recommend to Council:
  - (a) That the Town of Aurora enter into an agreement with the Lake Simcoe Region Conservation Authority on the East Holland River, Fish Barrier Removal, Restoration and Bridge Replacement project; and
  - (b) That staff be authorized to proceed with the preferred option to remove and replace the concrete culvert with a steel span bridge, salvaging the culvert for display opportunities.

**6. Informational Items**

**4. Memorandum from Planner**

**Re: Building Tension: When to tear down and when to build up**

**Recommended:**

1. That the memorandum regarding Building Tension: When to tear down and when to build up, be received for information.

**5. Extract from Council Meeting of February 13, 2018**

**Re: Summary of Committee Recommendations Report No. 2018-01**

**Recommended:**

1. That the Extract from Council meeting of February 13, 2018 regarding the Summary of Committee Recommendations Report No. 2018-01, be received for information.

**7. New Business**

**8. Adjournment**



**Town of Aurora  
Heritage Advisory Committee  
Meeting Minutes**

<b>Date:</b>	Monday, February 12, 2018
<b>Time and Location:</b>	7 p.m., Holland Room, Aurora Town Hall
<b>Committee Members:</b>	Councillor Wendy Gaertner (Chair), Councillor Jeff Thom (Vice Chair), Neil Asselin, Barry Bridgeford, James Hoyes, John Kazilis, and Bob McRoberts (Honorary Member) and Ken Turriff
<b>Members Absent:</b>	Martin Paivio
<b>Other Attendees:</b>	Marco Ramunno, Director of Planning and Development Services, Jeff Healey, Planner, and Ishita Soneji, Council/Committee Secretary

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The Chair called the meeting to order at 7 p.m.

**1. Approval of the Agenda**

**Moved by Councillor Thom  
Seconded by Ken Turriff**

That the agenda as circulated by Legislative Services, with the following addition, be approved:

- Delegation (b) Steve Mill, Resident; Re: Item 3 – HAC18-003 – Heritage Permit Application, Our Lady of Grace Church, 15347 Yonge Street and 16 Catherine Avenue, File: NE-HCD-HPA-18-01

**Carried**

**2. Declarations of Pecuniary Interest and General Nature Thereof**

There were no declarations of pecuniary interest under the *Municipal Conflict of Interest Act*.

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### **3. Receipt of the Minutes**

**Heritage Advisory Committee Meeting Minutes of December 11, 2017**

**Moved by John Kazilis**

**Seconded by Barry Bridgeford**

That the Heritage Advisory Committee meeting minutes of December 11, 2017, be received for information.

**Carried**

### **4. Delegations**

**(a) Sara Jordao, Larkin Architect Ltd. representing Our Lady of Grace Church**

**Re: Item 3 – HAC18-003 – Heritage Permit Application, Our Lady of Grace Church, 15347 Yonge Street and 16 Catherine Avenue,  
File: NE-HCD-HPA-18-01**

Ms. Jordao was present to respond to questions from the Committee, and provided any necessary clarifications regarding the application.

**Moved by John Kazilis**

**Seconded by Barry Bridgeford**

That the comments of the delegation be received and referred to Item 3.

**Carried**

**(b) Steve Mills, Resident**

**Re: Item 3 – HAC18-003 – Heritage Permit Application, Our Lady of Grace Church, 15347 Yonge Street and 16 Catherine Avenue,  
File: NE-HCD-HPA-18-01.**

Mr. Mills expressed concerns regarding the lack of coniferous trees and landscaping surrounding the proposed parking lot along Maple Street, and responded to questions.

**Moved by Bob McRoberts**

**Seconded by Councillor Thom**

That the comments of the delegation be received and referred to Item 3.

**Carried**

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## **5. Matters for Consideration**

The Committee consented to consider items in the following order: Item 3, 1 and 2.

### **1. HAC18-001 – Request for Letter of Support – Community Heritage Ontario**

Staff provided an overview and intent of the letter of support.

**Moved by Bob McRoberts**

**Seconded by Ken Turriff**

1. That Report No. HAC18-001 be received; and
2. That the Heritage Advisory Committee recommend to Council:
  - (a) That staff prepare a letter in support of Recommendations identified within Report 10 of the House of Commons Standing Committee on Environment and Sustainable Development.

**Carried**

### **2. HAC18-002 – Request to Remove a Property from the Aurora Register of Properties of Cultural Heritage Value or Interest 15858 Yonge Street**

Staff provided background and a brief overview of the application.

The Committee inquired about the demolition plans and inquired about the deliberation process at the Design Review Panel, and staff provided clarifications.

**Moved by Councillor Thom**

**Seconded by John Kazilis**

1. That Report No. HAC18-002 be received; and
2. That the Heritage Advisory Committee recommend to Council:
  - (a) That the property located at 15858 Yonge Street be removed from the Aurora Register of Properties of Cultural Heritage Value or Interest; and
  - (b) That future building elevations are subject to approval of Planning staff; and

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- (c) That future building elevations are subject to a second review by the Design Review Panel prior to issuance of a Building Permit.

**Carried**

**3. HAC18-003 – Heritage Permit Application**

**Our Lady of Grace Church, 15347 Yonge Street and 16  
Catherine Avenue, File: NE-HCD-HPA-18-01**

Staff provided a brief overview of the application and the proposed alterations.

The Committee and staff discussed various aspects of the application including the means of preservation and structural support of the heritage structures during demolition, need for an entrance from Yonge Street, various structural features including the steeple, and the need for low growing coniferous vegetation surrounding the parking lot along Maple Street.

**Moved by Neil Asselin**

**Seconded by Barry Bridgeford**

1. That Report No. HAC18-003 be received; and
2. That the Heritage Advisory Committee recommend to Council:
  - (a) That Heritage Permit Application NE-HCD-HPA-18-01 be approved; and
  - (b) That a front entrance, facing Yonge Street, be added to the new enclosed Narthex; and**
  - (c) That the location design and wording of commemorative heritage plaques be approved by staff prior to execution of the Site Plan Agreement; and
  - (d) That, prior to execution of the Site Plan Agreement, the owner submit a Letter of Credit to the Town for the installation of commemorative heritage plaques and the preservation of 16 Catherine Avenue during the demolition of Lynett Hall and construction of the proposed addition; **and**
  - (e) That the Site Plan Agreement ensure the inclusion of coniferous landscape screening, in keeping with the CPTED (Crime**



**Prevention Through Environmental Design) principles, to address the visual impact of the parking lot along Maple Street.**

**Carried as amended**

## **6. Informational Items**

### **4. HAC17-027 – Yonge Street Building Inventory**

Staff provided a brief overview of the report.

The Committee and staff discussed the possibility of conducting an objective evaluation of various properties located in the downtown core of Yonge Street, and suggested that the evaluations be brought back to the Committee for further review. The Committee further discussed the scope of work for the Evaluation Working Group during the evaluation process.

**Moved by Councillor Thom  
Seconded by Neil Asselin**

1. That Report No. HAC17-027 be received; and
2. That the Heritage Advisory Committee recommend to Council:
  - (a) That the Evaluation Working Group provide an objective evaluation on each of the following properties:
    - 15243B Yonge Street; "Whimster's Store"
    - 15243 A Yonge Street; "Mulock Block"
    - 15242 Yonge Street; "The Lloyd Building"
    - 15240 Yonge Street; "The Ashton Building"
    - 15233 Yonge Street; "Medical Hall"
    - 15229 Yonge Street; "The Willis Building"
    - 15225 Yonge Street; "Winter's Bakery"
    - 15222, 15224 & 15226 Yonge Street; "The Faughner Building"
    - 15221 Yonge Street; "Sterling Bank"
    - 15218-15220 Yonge Street; "The Clift Building"
    - 15216 Yonge Street; "The Odd Fellows Block"
    - 15210 Yonge Street; "The Andrews Block"
    - 15203 Yonge Street; "The Butcher Building"

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- **15199 Yonge Street; "The Grimshaw Bakery"**
- **15195 Yonge Street; and**

**(b) That the objective evaluations be submitted to the Heritage Advisory Committee meeting of May 14, 2018, for further review.**

**Carried as amended**

- 5. Extract from Council Meeting of January 30, 2018**  
**Re: Heritage Advisory Committee Meeting Minutes of November 13, 2017**

**Moved by James Hoyes**  
**Seconded by Bob McRoberts**

1. That the Extract from Council Meeting of January 30, 2018, regarding the Heritage Advisory Committee meeting minutes of November 13, 2017, be received for information.

**Carried**

## **7. New Business**

The Committee inquired about the mitigation of ongoing construction at 32 Wellington Street, and staff provided a response.

## **8. Adjournment**

**Moved by Neil Asselin**  
**Seconded by Councillor Thom**

That the meeting be adjourned at 9:48 p.m.

**Carried**

Committee recommendations are not binding on the Town unless otherwise adopted by Council.



**Town of Aurora**

**Heritage Advisory Committee Report No. HAC18-004**

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**Subject:** Request to Designate under Part IV of the *Ontario Heritage Act*  
19 & 21 Machell Avenue “The John van Nostrand House”

**Prepared by:** Jeff Healey, Planner/Heritage Planning

**Department:** Planning and Development Services

**Date:** March 5, 2018

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### Recommendation

1. That Report No. HAC18-004 be received; and
2. That the Heritage Advisory Committee recommend to Council:
  - a) *THAT the House located at 19-21 Machell Avenue be designated under Part IV of the Ontario Heritage Act as a property of cultural heritage value or interest; and*
  - b) *THAT the Town Clerk be authorized to publish and serve Council’s Notice of Intention to Designate as per requirements of the Act; and*
  - c) *THAT the designation by-law be brought before Council for passing if no objections were received within the thirty (30) day objection period as per requirements of the Act; and*
  - d) *THAT the owners of 19 & 21 Machell Ave be thanked for their support of the designation of the subject heritage property.*

### Executive Summary

The purpose of this report is to provide the Heritage Advisory Committee with all background materials so it can recommend to Council that the house located at 19 and 21 Machell Avenue be designated as a property of cultural heritage value or interest under Section 29 (Part IV) of the *Ontario Heritage Act* for its cultural heritage value and interest.

## **Background**

### **Location**

The subject property is located on the east side of Machell Avenue between Wellington Street East and Irwin Avenue (see Attachment 1). The property is currently listed and non-designated on the Aurora Register of Properties of Cultural Heritage Value or Interest and can be described as a 2 storey Ontario House.

### **Existing Policy Context**

#### *Ontario Heritage Act*

The *Ontario Heritage Act* provides municipalities the ability to protect significant Cultural Heritage Resources within its jurisdiction. Conservation measures under the *Ontario Heritage Act* are outlined in Section 33 of the *Act*:

*33. (1) No owner of property designated under section 29 shall alter the property or permit the alteration of the property if the alteration is likely to affect the property's heritage attributes, as set out in the description of the property's heritage attributes that was required to be served and registered under subsection 29 (6) or (14), as the case may be, unless the owner applies to the council of the municipality in which the property is situate and receives consent in writing to the alteration*

Municipal processes within the Town of Aurora require the owner to submit a Heritage Permit application should a request to alter the property under Section 33 of the *Ontario Heritage Act* be requested by the owner. Approval of a Heritage Permit is provided either by Council or through staff (via delegation By-law 5365-11).

#### *Provincial Policy Statement (2014)*

The Provincial Policy Statement (PPS) provides policy direction on matters of provincial interest. The PPS identifies that significant built heritage resources and significant cultural heritage landscapes shall be conserved.

#### *York Region Official Plan*

The York Region Official Plan requires local municipalities to conserve significant cultural heritage resources and ensure that development and site alteration of adjacent

lands to protected heritage properties will conserve the heritage attributes of the protected heritage property.

### *Official Plan*

Section 13 of the Official Plan outlines the policies for conserving Cultural Heritage Resources in the Town of Aurora. The Official Plan states that all significant heritage resources shall be designated as being of cultural heritage value or interest in accordance with the *Ontario Heritage Act* to ensure effective protection and their continuing maintenance, conservation and restoration. Evaluation Criteria for assessing the cultural heritage value will include: the aesthetic design or physical value; the historical or associative value; and/or the contextual value of the property. Furthermore, the plan states that heritage resources will be conserved in accordance with the Standards and Guidelines for the Conservation of Historic Places in Canada, the Appleton Charter for the Protection and Enhancement of the Built Environment and other recognized heritage protocols and standards.

## **Analysis**

Attachment #2 provides a summary of the Architectural, Historical and Contextual value of 19-21 Machell Avenue. The proposed Statement of Cultural Heritage Value or Interest is provided below:

*Built circa 1885; the John van Nostrand house is an early example of a purpose built semi-detached structure constructed in Aurora in the late 19<sup>th</sup> century. 19-21 Marchell is a 2 ½ storey brick structure with a cross gable roof, peaked centre gable and clad in brick. The building is designed in an Ontario Gothic Revival Architectural style with influence of Italianate design.*

*The house is associated with Mr. John van Nostrand, who was a timber lot and sawmill owner in Vandorf, east of Aurora. Mr. van Nostrand (and later his estate) tenanted the home during his 70-year ownership. One notable tenant was Matthew Hind Thompson who was the first Principal of Church Street School, lived in the home with his family between 1890 and 1909.*

*19-21 Machell Avenue is prominently located midway along Machell Avenue in the vicinity of other historic homes. This house and others nearby help to create a sense of heritage character in this part of Old Aurora.*

## **Heritage Building Evaluation**

The purpose of the Heritage Building Evaluation is to identify the design/physical value, historical/associative value, and contextual value of the property as per Ontario Regulation 9/06, which outlines the criteria for determining cultural heritage value or Interest under the *Ontario Heritage Act* in order to conserve significant heritage resources.

The Evaluation Working Group met to perform an objective evaluation of the subject property on Wednesday February 14, 2018 (See Attachment 3). The Evaluation Criteria for assessing the cultural heritage value of cultural heritage resources have been developed by the Town in consultation with its Municipal Heritage Committee. As per Section 13.3 e) of the Official Plan, Priority will be given to designating all Group 1 heritage resources in the Register.

The Evaluation found the subject property to score in the high end of Group 2, suggesting that the property is “significant, worthy of preservation”.

According to the Heritage Evaluation Guide for buildings scored within Group 2:

- The designation of the building pursuant to the *Ontario Heritage Act* will be encouraged;
- The retention of the structure in its existing location is encouraged;
- Any development application affecting such a structure should incorporate the identified building; and
- Appropriate alternative uses for the building will be encouraged when necessary to ensure its preservation.
- A Letter of Credit may be required to ensure the protection and preservation of the building in connection with a redevelopment application.

The *Ontario Heritage Act* provides criteria for determining cultural heritage value or interest with Ontario Regulation 9/06. This Regulation requires that a building must exhibit significant design/physical, or associative, or contextual value to warrant designation. The Evaluation working group found the final weighted score for 19-21 Machell Avenue to be 69.5/100. It is noted that a score of 70/100 would meet the minimum threshold of Group 1.

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## **Legal Considerations**

N/A

## **Financial Implications**

N/A

## **Communications Considerations**

Approval of this report will authorize the Town Clerk to publish and serve Council's Notice of Intention to Designate as per requirements of the *Ontario Heritage Act*, including notice in the local newspaper.

## **Link to Strategic Plan**

The conservation of heritage resources supports the Strategic Plan goal of ***Supporting an Exceptional Quality of Life for All*** through its accomplishment in satisfying requirements in objective ***Celebrating and Promoting our Culture***.

## **Alternative(s) to the Recommendation**

1. That Designation under Part IV of the *Ontario Heritage Act* not be pursued.

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## **Conclusions**

The house located at 19-21 Machell Avenue is a significant heritage resource in Aurora and worthy of designation under Part IV of the *Ontario Heritage Act*. The designation of the subject property will help in the preservation of the structure and its significance in Aurora.

## **Attachments**

Attachment 1 – Location Map

Attachment 2 – Heritage Designation Brief – 19-21 Machell Ave

Attachment 3 – Heritage Building Evaluation – 19-21 Machell Ave

Attachment 4 – Heritage Resource Brief (2010)

## **Previous Reports**

None.

## **Pre-submission Review**

Agenda Management Meeting review on February 22, 2018

## **Departmental Approval**



**Marco Ramunno**

**Director**

**Planning and Development Services**





**LOCATION MAP**

HAC18-004  
19 & 21 MACHELL AVENUE

 **SUBJECT LANDS**



**Attachment #2**

**Heritage Designation Brief**



**John van Nostrand House**

**Circa 1885**

**19 & 21 Machell Avenue**

**Aurora, ON**

**March 2018**

**HERITAGE PROPERTY STATUS SHEET**

Street address: 19 & 21 Machell Avenue  
Roll number: 1946-000-010-93600-0000  
Short Legal description: Lot 5, Plan 36, Town of Aurora, Regional Municipality of York, being all of PIN 03637-0340 (LT)  
Year built: Between 1882 and 1888  
Original Owner(s): John van Nostrand  
Current Owner(s): Robert Worthman & Bernadette Cwenar  
House name: John van Nostrand House  
Builder: Unknown  
Owner's concurrence: Yes  
Original use: Residential  
Current use: Residential  
Heritage status: Listed property, Aurora Register of Properties of Cultural Heritage Value or Interest  
Reasons for report: Designation - Planning application  
Heritage Brief Completion Date: March 2018  
Prepared by: Jeff Healey, Planner/Heritage Planning  
Historical research: Jacqueline Stuart  
Submission date: March 5, 2018  
Report number: HAC18-004

### **Location of the Property**

The John van Nostrand House at 19 & 21 Machell Avenue, Town of Aurora, is located on the east side of Machell Avenue, between Wellington Street East to the south and Irwin Avenue to the north.

The property has 19.8 metres of frontage on Machell Avenue with approximate lot area of 809.1 square metres. The boundary of the property is legally described as: Lot 5, Plan 36, Town of Aurora, Regional Municipality of York, being all of PIN 03637-0340 (LT)

### **Statement of Cultural Heritage Value or Interest**

Built circa 1885; the John van Nostrand house is an early example of a purpose built semi-detached structure constructed in Aurora in the late 19<sup>th</sup> century. 19-21 Marchell is a 2 ½ storey brick structure with a cross gable roof, peaked centre gable and clad in brick. The building is designed in an Ontario Gothic Revival Architectural style with influence of Italianate design.

The house is associated with Mr. John van Nostrand, who was a timber lot and sawmill owner in Vandorf, east of Aurora. Mr. van Nostrand (and later his estate) tenanted the home during his 70-year ownership. One notable tenant was Matthew Hind Thompson who was the first Principal of Church Street School, lived in the home with his family between 1890 and 1909.

19-21 Machell Avenue is prominently located midway along Machell Avenue in the vicinity of other historic homes. This house and others nearby help to create a sense of heritage character in this part of Old Aurora.

### **Assessment of Cultural Heritage Value or Interest**

#### **Historical or Associative Value**

There is no strong clue as to the year of construction of the semi-detached dwelling at 19–21 Machell Avenue has been found. However it appears that the building at 19–21 Machell was erected possibly as early as 1882 and certainly by 1888.

The purchaser of the four one-fifth acre lots was John van Nostrand. Mr. van Nostrand was a timber lot and sawmill owner in Vandorf, east of Aurora; the name of that community comes from his surname. Like many in that village he had strong business and social links with Aurora. Mr. van Nostrand owned 19-21 Machell and the four adjacent lots. He (or, later, his estate) would sell lots 7 and 8 in 1882, most of lot 6 in 1945, and the rest of lot 6 and lot 5, the property in question, in 1949.

The two units and their associated open spaces are more or less identical and for decades had the same owner: pairs of almost identical rented properties in the tax records may well refer to number 19–21.

One long-term tenant of number 19 or number 21 was Matthew Hind Thompson, who lived here with his family from at least 1890 until 1909. As street numbers were not used at the time we do not know which half of the semi-detached dwelling housed the Thompsons. Mr. Thompson was born in England in 1846 but came to Canada as a young boy with his family. As a young man he worked first as a druggist in Victoria County: he and Sophia were married in Lindsay. In 1877 Mr. Thompson was still a druggist in Fenelon Falls, but by at least 1880, however, he was employed as a teacher in King township. When the fine new public school building was opened on Church Street in Aurora in 1886 Matthew H. Thompson was its principal. Mr. Thompson resigned from his position late in 1910 to take up a post within the department of education, and the family moved to Toronto.

From at least 1930 until possibly 1952 the homes at number 19 and number 21 Machell Avenue were occupied by various combinations of the McGhee family. The head of the family was James McGhee, a Scottish widower who emigrated to Canada in 1924. He had worked in a shoe factory in Scotland and would do the same work here. Mr. McGhee's Aurora employer would almost certainly have been the T. Sisman Shoe Company. When James McGhee immigrated in 1924 he was accompanied by his daughter and son, Margaret and Thomas McGhee. His daughter, Helen, would follow in 1927.

After seventy-two years of John van Nostrand ownership, lot 5 was sold in 1949, but it remained a rental property.

Following two more changes of ownership, the lot was severed in 1952. Late in that year Sarah Hazel Robertson (Mrs. Francis Robertson) bought the south half, number 19. Early in 1953 Jack Douglas Ranson, a chef, purchased the north half, or number 21. The north half changed hands again in 1956 but was acquired by Mrs. Robertson in 1967: once again the two parts of the lot were under the same ownership. Throughout this period the house was actually occupied, at least in part, by its owners.

Mrs. Robertson sold lot 5 to Mrs. Lois Creelman in 1976 and it remained in her ownership, once again as a rental property, until 2016.

## **Architectural Value**

19-21 Machell is a 2 ½ storey brick structure with a cross gable roof, peaked centre gable with stone foundation and clad in beige brick. The building is designed in an Ontario Gothic Revival Architectural style, however carries some Italianate inspiration hinted by the presence of a cornice and two brackets on top of the box windows on the front elevation.

Purposely built as semidetached structure, this house pattern is a rare example of this housing type among 19<sup>th</sup> Century housing stock in Aurora. The building is perfectly symmetrical in both building size and openings.

The front doors are centre on the building, giving the impression of a single entrance to the building. A wood porch spans across both front doors, with independent entrances and separated at the centre by a wood screen. The front elevation is a mirror image between the north and south- matching box window capped with a cornice with brackets on the first story, four balanced two-over-two windows on the second floor, two windows per dwelling unit.

The matching characteristics of the building carries into the side and rear elevations, featuring identical window and door openings. Original window and door openings on all elevations feature a segmental arch with a brick voussoir.

A wood air vent with a round arch is centred underneath the peaked front gable. The vent is capped with a brick voussoir. The centre gable features decorative gingerbread bargeboard

### **Contextual Value**

19-21 Machell Avenue is prominently located midway along Machell Avenue in the vicinity of other historic homes. This house and others nearby help to create a sense of heritage character in this part of Old Aurora.

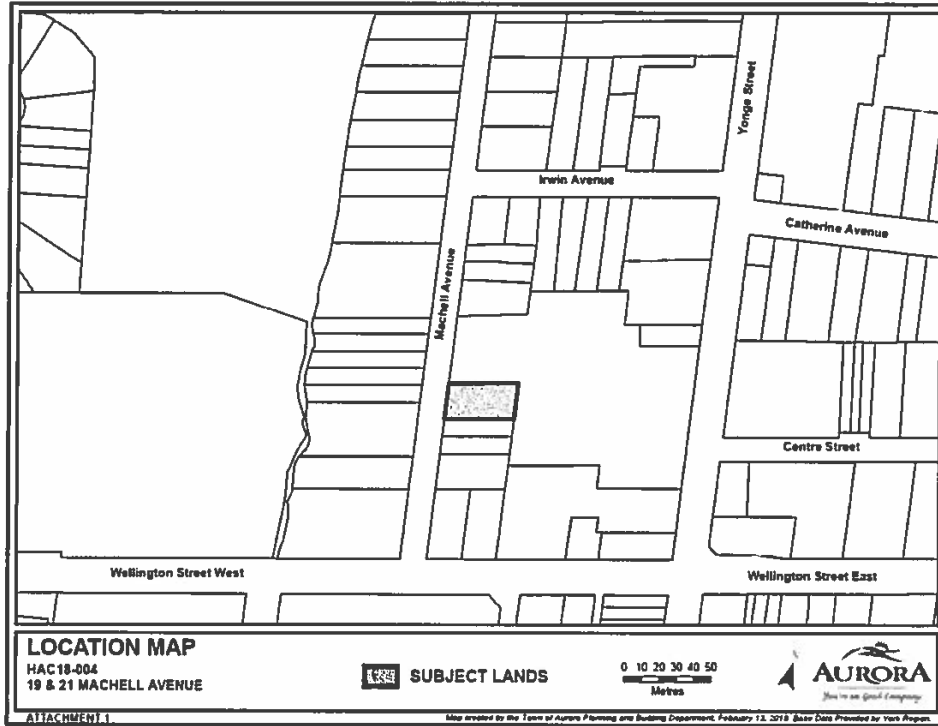
### **Summary of Heritage Attributes**

Reasons for Designation include the following heritage attributes and apply to all elevations and the roof including all facades, entrances, windows and trim, together with construction materials, their related building techniques and landscape features.

### **Exterior Elements**

- The overall 2 ½ storey house with beige brick exterior
- Cross gable roof
- Gingerbread bargeboard trim
- All window and door openings
- 2 over 2 wood sash windows and trim

## Maps and photos



Location map – 19-21 Machell Avenue, Aurora.  
Source: Town of Aurora



Fire Insurance Map, Town of Aurora, showing the subject property as 2½ storey main structures with 1 storey addition at rear, the main structure is framed with brick veneer. Surveyed March 1904, Revised to November 1913 by Chas E. Goad Co., Civil Engineers. Source: Aurora Historical Society.



19-21 Machell Avenue – Front elevation (east), Photo 2010  
Source: Town of Aurora





19-21 Machell Avenue – Front elevation (east), Photo 2018  
Source: Town of Aurora



19-21 Machell Avenue – Rear elevation (west), Photo 2018  
Source: Town of Aurora



**19-21 Machell Avenue – Side elevation (north), Photo 2018**  
Source: Town of Aurora



**19-21 Machell Avenue – Side elevation (south), Photo 2018**  
Source: Town of Aurora



19-21 Machell Avenue –Gingerbread Bargeboard under centre gable - Photo 2018  
Source: Town of Aurora



19-21 Machell Avenue – 2 over 2 wood sash windows, Photo 2018  
Source: Town of Aurora

**HERITAGE BUILDING EVALUATION SCORE SHEET**

Municipal Address: <u>19-21 Macchell Ave</u>					Group: <u>2</u>
Legal Description: <u>W000000000 Plan 36</u>		Lot: <u>5</u>	Cons: _____		
Date of Evaluation: <u>Feb 14<sup>th</sup>/18</u>		Name of Recorder: <u>JH</u>			
<b>HISTORICAL</b>	<b>E</b>	<b>G</b>	<b>F</b>	<b>P</b>	<b>TOTAL</b>
Date of Construction	30	<u>20</u>	10	0	20/30
Trends/Patterns/Themes	40	<u>27</u>	14	0	27/40
Events	15	10	5	<u>0</u>	0/15
Persons/Groups	<u>15</u>	10	5	0	15/15
Archaeological (Bonus)	10	7	<u>3</u>	0	3/10
Historic Grouping (Bonus)	10	7	3	<u>0</u>	/10
Construction Date (Bonus)	10				/10
<b>HISTORICAL TOTAL</b>					<b>65/100</b>
<b>ARCHITECTURAL</b>	<b>E</b>	<b>G</b>	<b>F</b>	<b>P</b>	<b>TOTAL</b>
Design	20	<u>13</u>	7	0	13/20
Style	<u>30</u>	20	10	0	30/30
Architectural Integrity	<u>20</u>	13	7	0	20/20
Physical Condition	<u>20</u>	13	7	0	20/20
Design/Builder	10	7	3	<u>0</u>	0/10
Interior (Bonus)	10	7	3	0	/10
<b>ARCHITECTURAL TOTAL</b>					<b>83/100</b>
<b>ENVIRONMENTAL</b>					<b>TOTAL</b>
Design Compatibility	40	<u>27</u>	14	0	27/40
Community Context	20	13	<u>7</u>	0	7/20
Landmark	20	13	<u>7</u>	0	7/20
Site	<u>20</u>	13	7	0	20/20
<b>ENVIRONMENTAL TOTAL</b>					<b>61/100</b>

SCORE	INDIVIDUAL	OLD AURORA
Historical Score	X 40% = _____	65 X 20% = <u>13</u>
Architectural Score	X 40% = _____	83 X 35% = <u>29.05</u>
Environmental Score	X 20% = _____	61 X 45% = <u>27.45</u>
<b>TOTAL SCORE</b>	<input type="text"/>	<b>69.5</b>


GROUP 1 = 70-100

**GROUP 2 = 45-69**

GROUP 3 = 44 or less



**AURORA REGISTER OF PROPERTIES OF CULTURAL  
HERITAGE VALUE OR INTEREST (Updated 2017)**

<b>SITE</b>	<p>Address: 19-21 Machell Avenue          Former Address:          Legal Description: PLAN: 36 PART LOT: 5</p>
<b>STATUS</b>	<p>Current Use: Residence (semi-detached) Original use: Residence (semi-detached)          Heritage Status: Listed By-law No. &amp; Date:          Official Plan: Aurora Promenade Zoning: PD2(473)          HCD: Plaques:</p>
<b>PHOTOGRAPH</b>	
<b>KEY MAP</b>	

**AURORA REGISTER OF PROPERTIES OF CULTURAL  
HERITAGE VALUE OR INTEREST (Updated 2017)**

**ARCHITECTURE**

**GENERAL INFORMATION:**

<b>Address:</b>	19-21 Machell Avenue	<b>Builder:</b>	
<b>Construction Date:</b>	C1880	<b>Architect:</b>	
<b>Architectural Style:</b>	Ontario House	<b>Original Owner:</b>	John van Nostrand
<b>Heritage Easement:</b>		<b>Historical Name:</b>	

**GENERAL DESCRIPTION:**

<b>Floor Plan:</b>		<b>Storey:</b>	2
<b>Foundation Materials:</b>			
<b>Exterior Wall Materials:</b>			
<b>Roof Type:</b>	Gable; centre gable with keyhole ventilator	<b>Windows:</b>	2 bracketed box bay
<b>Entrance:</b>		<b>Bays:</b>	

**UNIQUE FEATURES:**

<b>Chimney (s):</b>		<b>Special Windows:</b>	
<b>Dormers:</b>		<b>Porch/Verandah:</b>	
<b>Roof Trim:</b>		<b>Door Trim:</b>	
<b>Window Trim:</b>		<b>Other:</b>	

**HISTORY**

**Historical Society files include:**

**Town of Aurora files include:**

**PHOTOS:**

<b>HISTORICAL PHOTO</b>	<b>1995 INVENTORY PHOTO</b>
Photo date	Photo date

The Aurora Inventory of Heritage Buildings was compiled by the Aurora Heritage Advisory Committee (LACAC) between 1976 and 1981. The completed inventory was adopted by Council and released in 1981. On September 26, 2006 Aurora Council at its meeting No. 06-25, has officially changed the name of the Aurora Inventory of Heritage Building to the "*Aurora Register of Property of Cultural Heritage Value or Interest*" and all property included in the Inventory were transferred to the Register.



**Town of Aurora**

**Heritage Advisory Committee Report No. HAC18-005**

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**Subject:** Additional Information: Request to Remove a Property from the Aurora Register of Properties of Cultural Heritage Value or Interest  
14452 Yonge Street

**Prepared by:** Jeff Healey, Planner/Heritage Planning

**Department:** Planning and Development Services

**Date:** March 5, 2018

---

## **Recommendation**

1. That Report No. HAC18-005 be received; and
2. That the Heritage Advisory Committee recommend to Council:
  - a) That 14452 Yonge Street be removed from the Aurora Register of Properties of Cultural Heritage Value or Interest; and
  - b) That a financial contribution of \$75,000.00 be provided to the Town's Heritage Reserve Fund;

## **Executive Summary**

The purpose of this report is to provide Council with direction from the Heritage Advisory Committee regarding the request to demolish all buildings and structures located at 14452 Yonge Street. The property is currently Listed on the Aurora Register of Properties of Cultural Heritage Value or Interest.

- The house on the subject lands was constructed circa 1930
- The Heritage Evaluation Working Group re-evaluated the property. The property remains in in the low end of Group 2 despite receiving a lower score.

## **Background**

In August 2016, the owner submitted a Plan of Subdivision, a Plan of Condominium and a Zoning By-law Amendment to the Town for the subject lands. The owner is proposing to construct 40 residential lots on the west half of the lands. On February 13, 2017 the Heritage Advisory Committee reviewed the request to remove the property from the Heritage Registrar and proved the following recommendation to Council:

March 5, 2018

Page 2 of 5

Report No. HAC18-005

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1. *That Report No. HAC17-002 be deferred; and*
2. *That the Heritage Advisory Committee recommend to Council: (a) That a structural report and re-evaluation of the property be brought back to a future Heritage Advisory Committee meeting.*

In February 2017, the owner has submitted a preliminary structural investigation for the main building, prepared by Delvin Engineering and has requested that the main building be re-evaluated by the Heritage Evaluation Working Group.

## **Analysis**

The Evaluation Working Group met to re-evaluate the subject property on Wednesday February 14, 2018 (See Attachment 5).

The Evaluation Working Group considered the submitted structural report, and changed the Physical Condition category from “Good” to “Fair” as it was determined that the existing building is not structurally compromised, however requires a moderate amount of structural repair.

It is noted that the first evaluation by the Evaluation Working Group in October 2016 was conducted prior to the site visit which occurred in January 2017. With the site visit taken into consideration, the Design category was adjusted from “Excellent” to “Good”. The fieldstone exterior finish is a stone facing rather than a true stone structure. Although the building remains a rare example of the bungalow architectural style within Aurora, the building does not exhibit a high degree of craftsmanship or design.

Finally the Evaluation Working Group reviewed the Architectural Integrity category and determined that a change in scoring was not necessary. Although a number of dormers and a 2<sup>nd</sup> floor balcony have been installed on the building, it would be relatively simple to remove these features if the building was to be restored to an original state.

The conclusion of the re-evaluation found the subject property to score at the low end of Group 2, suggesting that the property is “significant, worthy of preservation”.

According to the Heritage Evaluation Guide for buildings scored within Group 2:

- The designation of the building pursuant to the *Ontario Heritage Act* will be encouraged;
- The retention of the structure in its existing location is encouraged;



- Any development application affecting such a structure should incorporate the identified building; and
- Appropriate alternative uses for the building will be encouraged when necessary to ensure its preservation.

The conservation of remaining physical attributes of the property would require formal designation under Part IV of the *Ontario Heritage Act*, making it necessary for owners to obtain Heritage Permits for proposed work.

The *Ontario Heritage Act* provides criteria for determining cultural heritage value or interest with Ontario Regulation 9/06. This Regulation requires that a building must exhibit significant design/physical, or associative, or contextual value to warrant designation. The Evaluation working group found the revised final score for 14452 Yonge Street to be 46.6/100. The original final score was 51.8/100.

### **Proposed Concept Plan**

The owner did not provide a composite drawing as part of the previous report to the Heritage Advisory Committee. The owner has provided two composite models of a home proposed to be built in place of the existing building on the subject lands. A picture of the model home is shown on Attachment #6.

Staff have requested that a financial contribution to the Town's Heritage Reserve Fund be provided in the related Subdivision Agreement.

### **Legal Considerations**

N/A

### **Financial Implications**

None.

### **Communications Considerations**

N/A

## **Link to Strategic Plan**

The conservation of heritage resources supports the Strategic Plan goal of ***Supporting an Exceptional Quality of Life for All*** through its accomplishment in satisfying requirements in objective ***Celebrating and Promoting our Culture***.

## **Alternative(s) to the Recommendation**

1. That the owner be requested to consider options for preservation of the main building within the proposed plan of subdivision.

## **Conclusions**

The subject building was evaluated using the Town of Aurora Heritage Building Evaluation Guide and was rated in the low end of Group 2, which encourages the retention of the building as well as designation under the *Ontario Heritage Act*. Staff recommend the Heritage Advisory Committee remove of 14452 Yonge Street from the Aurora Registrar of Properties of Cultural Heritage Value or Interest.

March 5, 2018

Page 5 of 5

Report No. HAC18-005

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## **Attachments**

Attachment #1 – Location Map

Attachment #2 – Heritage Resource Brief (2010)

Attachment #3 – Preliminary Structural Investigation, prepared by Delvin Engineering

Attachment #4 – Revised Evaluation Working Group Score, 14452 Yonge Street

Attachment #5 – Original Evaluation Working Group Score, 14452 Yonge Street

Attachment #6 – Composite of proposed structure

## **Previous Reports**

1. Memorandum from the Program Manager, Heritage Planning Re: 14452 Yonge Street Research, dated June 9, 2014; and
2. Heritage Advisory Committee Report No. HAC17-002, dated February 13, 2017.

## **Pre-submission Review**

Agenda Management Meeting review on February 22, 2018

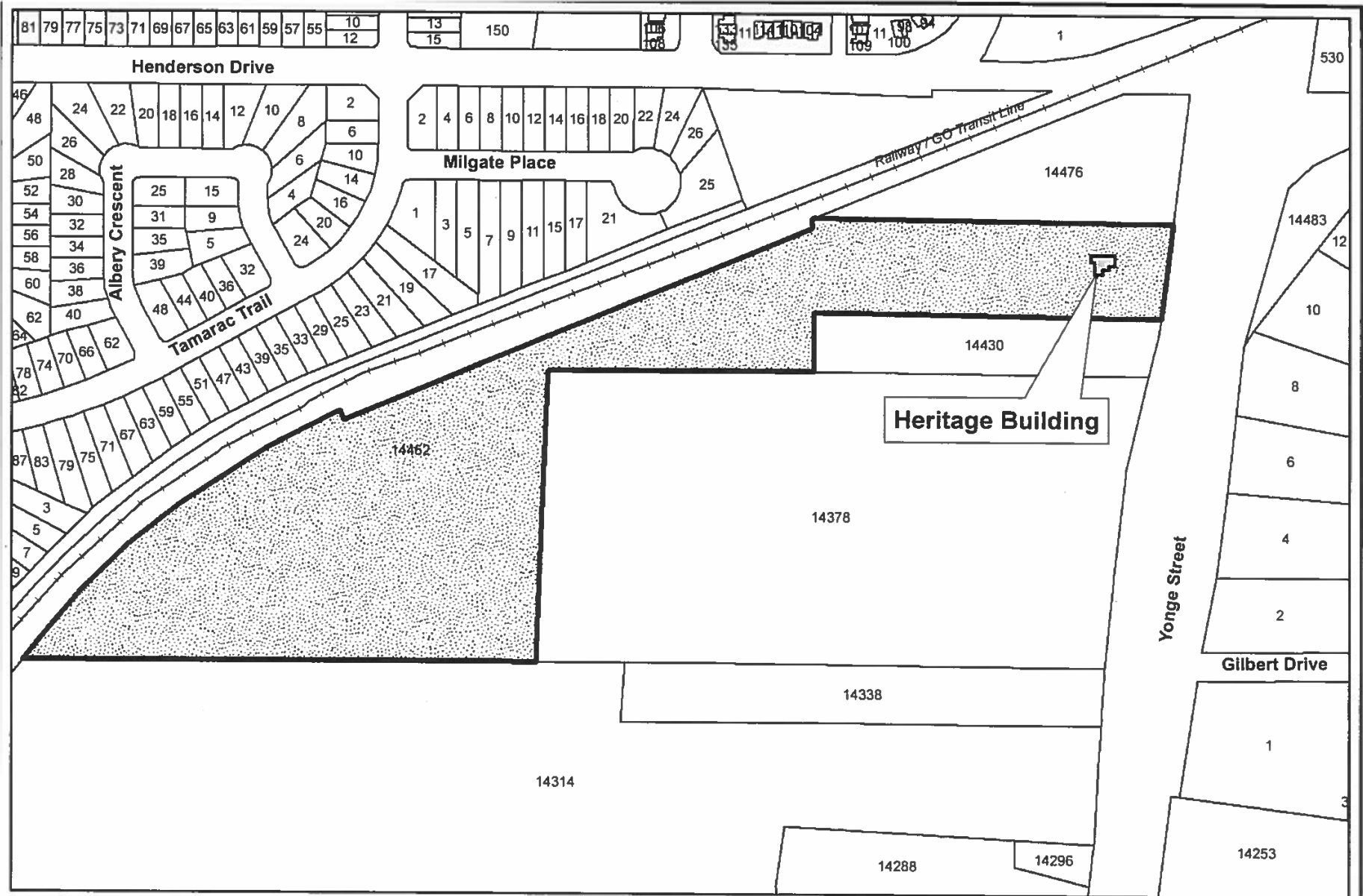
## **Departmental Approval**



**Marco Ramunno**

**Director**

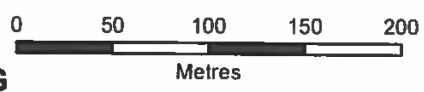
**Planning and Development Services**



**LOCATION PLAN**

HAC17-002  
 14452 YONGE STREET  
 ATTACHMENT 1

-  SUBJECT LANDS
-  LOCATION OF HERITAGE BUILDING



Map created by the Town of Aurora Planning and Building Department, January 30, 2017. Base Data Provided by York Region.

## AURORA INVENTORY OF HERITAGE BUILDINGS

**S  
I  
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E**

ADDRESS: 14452 Yonge Street (At the very end of 1400 meter R.O.W.)

**LEGAL DESCRIPTION:** CONCESSION: 1W PART LOT: 75

**S  
T  
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PRESENT USE: Residence

ORIGINAL USE: Residence

HERITAGE DESIGNATION: Undesignated

1995 INVENTORY

OFFICIAL PLAN: Rural

ZONING: (RU) Rural

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Devlin Engineering Ltd.  
15146 Kennedy Road  
Stouffville, ON  
L4A 4B8  
info@devlinengineering.com

February 4, 2018

Ballymore Building (South Aurora) Corp.  
12840 Yonge St., Suite 200  
Richmond Hill, Ontario  
L4E 4H1  
**Attention: Mr. Larry Dekkema**

**Re: Preliminary Structural Investigation of the Existing Residential Building Located at 14452  
Yonge St., Aurora; Project No. 180106.**

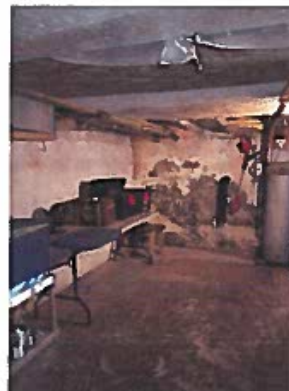
Dear Larry,

In accordance with your instructions, Devlin Engineering Ltd. carried out a site review on January 30, 2018 of the visually exposed structural elements of the existing building located at the above noted address. The following observations should be noted:

1. The existing house is a one and a half storey building constructed of a wood framing structural support system with an exterior stone/block veneer cladding system bearing on a cast-in-place concrete foundation wall system.
2. The existing cast-in-place concrete foundation walls, accommodating the basement area, has two existing window openings located on the west side of the building that have been abandoned and filled with a wood framing system (and backfilled on the exterior) [refer to Photo #1]; these wood fill walls have significantly deteriorated and require replacement (with a masonry fill wall system). Signs of water penetration through the existing foundation walls were noted at several locations [refer to Photo #2]; a new drainage/waterproofing system is recommended around the exterior perimeter of the foundation wall system.



**Photo #1: West Foundation Wall**  
*(rotted wood/water leakage)*



**Photo #2: East Foundation Wall**  
*(water leakage)*

3. The main floor framing system consists of 2"x 6" floor joists (complete with a wood planking sheathing system) bearing on exterior concrete foundation walls and interior load bearing walls. It was noted that the existing access openings (doors) through interior load bearing walls do not have support lintels; this is structurally unacceptable. In general, the existing main floor framing system is questionable and should be structurally analyzed and reinforced accordingly.
4. The existing exterior front porch, located at the main entrance on the east side of the building, has deteriorated and lacks adequate structural support; the existing front porch must be replaced [refer to Photos #3 & #4].



**Photo #3**  
Exterior Front Porch  
*(general view of steps)*



**Photo #4**  
Exterior Front Porch  
*(view of existing support members)*

5. The back entrance vestibule, located on the west side of the building, is a one-storey structure attached to the house (no basement). This existing vestibule is significantly deteriorated and hence structurally unacceptable; the back entrance vestibule must be replaced [refer to Photos #5 & #6].



**Photo #5**  
Back Entrance Vestibule  
*(general view of side elevation)*



**Photo #6**  
Back Entrance Vestibule  
*(general view of door entrance)*



6. The second floor framing system was not visually exposed at the time of this site visit and hence not structurally review. It should be noted that the second floor sheathing system appeared deformed (wavy) in some areas.
7. The second floor exterior balcony, located on the north side of the building, is structurally unacceptable and must be replaced [refer to Photo #7]. .



**Photo#7: Second Floor Exterior Balcony (*general view*)**

8. The existing roof is constructed of a wood stick-frame system; the underside of the roof rafter members are finished with drywall accommodating the second floor occupancy area. The roof framing members were not visually exposed at the time of this site visit and hence not structurally reviewed.
9. The existing roof framing system appears to have been structurally modified from original construction; the existing roof dormer located on the north side of the building has been added [dormer shown in Photo #7] and other areas of the roof framing system appear to have been modified. Several water stains were noted on the second floor ceil (underside of roof framing elevation) suggesting that water penetration (leak) through the existing roof's waterproofing system is occurring or has occurred in the past; potential wood rot and mold location [refer to Photos #8 & #9]. In general, the existing roof framing system is questionable and should be structurally analyzed and reinforced accordingly.



**Photo #8**  
Second Floor Ceiling Elevation  
*(leaks in corridor)*



**Photo #9**  
Second Floor Ceiling Elevation  
*(leaks at chimney)*

10. The existing exterior walls are constructed of a 4" thick stone/masonry block veneer system (single wythe) complete with a wood support backing wall system; no access was available to the backing wall framing system at the time of this site review. The existing exterior stone walls are significantly deteriorated and have cracked and spalled (missing stones) at many locations located around the perimeter of the building; existing mortar joints have typically cracked and failed around the perimeter of the building. As a result of these cracks and openings through the existing exterior stone veneer wall system, it appears that significant water penetration, through the stone walls, has been occurring over the past years; it is likely that the wood backing wall is rotted (mold may also exist at this location); no access to the backing wall was available at the time of this site review [refer to Photos #10 to #13]. In general, the existing exterior stone cladding system should be replaced and the load bearing wood backing wall system should be reviewed accordingly.



**Photo #10**  
North/East Corner  
*(masonry block veneer at corners; typical)*



**Photo #11**  
South Elevation  
*(previous repairs; open cracks)*



**Photo #11**  
North Elevation  
*(missing stone at grade elevation;  
typical at several locations)*



**Photo #12**  
North Elevation  
*(general view; saturated stones at grade  
elevation; typical)*

11. All exterior window and door frames and assemblies have deteriorated and require replacement. The roof shingles were covered with snow at the time of the site visit and hence not reviewed; the eaves and flashing around the perimeter of the building have typically failed and should be replaced.

Read this report in conjunction with all other project documents. If you require any additional information or clarification please feel free to contact this office.

Sincerely,  
Devlin Engineering Ltd.



A handwritten signature in black ink, appearing to read "Martin Devlin".

Martin Devlin, M.Eng., P. Eng.

**HERITAGE BUILDING EVALUATION SCORE SHEET**

Municipal Address: <u>14452 Yonge Street</u>					Group: <u>2</u>
Legal Description: _____		Lot: _____		Cons: _____	
Date of Evaluation: <u>Feb 14/2018</u> <i>Re-evaluation</i>		Name of Recorder: <u>JH</u>			
<b>HISTORICAL</b>	<b>E</b>	<b>G</b>	<b>F</b>	<b>P</b>	<b>TOTAL</b>
Date of Construction	30	20	<u>10</u>	0	10/30
Trends/Patterns/Themes	40	<u>27</u>	14	0	27/40
Events	15	10	5	<u>0</u>	0/15
Persons/Groups	15	10	5	<u>0</u>	0/15
Archaeological (Bonus)	10	7	3	<u>0</u>	0/10
Historic Grouping (Bonus)	10	7	<u>3</u>	0	30/10
Construction Date (Bonus)	10				0/10
<b>HISTORICAL TOTAL</b>					<b>40/100</b>
<b>ARCHITECTURAL</b>	<b>E</b>	<b>G</b>	<b>F</b>	<b>P</b>	<b>TOTAL</b>
* Design	<del>20</del>	<u>13</u>	7	0	13/20
Style	30	20	<u>10</u>	0	10/30
Architectural Integrity	20	<u>13</u>	7	0	13/20
* Physical Condition	20	<del>15</del>	<u>7</u>	0	7/13
Design/Builder	10	7	<u>3</u>	0	3/10
Interior (Bonus)	10	<u>7</u>	3	0	7/10
<b>ARCHITECTURAL TOTAL</b>					<b>53/66</b>
<b>ENVIRONMENTAL</b>					<b>TOTAL</b>
Design Compatibility	40	<u>27</u>	14	0	27/40
Community Context	20	15	7	<u>0</u>	0/20
Landmark	20	13	7	<u>0</u>	0/20
Site	<u>20</u>	13	7	0	20/20
<b>ENVIRONMENTAL TOTAL</b>					<b>47/100</b>
<b>SCORE</b>	<b>INDIVIDUAL</b>		<b>OLD AURORA</b>		
Historical Score	40 X 40% = <u>16</u>		X 20% = _____		
Architectural Score	53/66 X 40% = <u>21.2</u>		X 35% = _____		
Environmental Score	47 X 20% = <u>9.4</u>		X 45% = _____		
<b>TOTAL SCORE</b>	<b>51.8</b> <u>46.6</u>		<input type="text"/>		
GROUP 1 = 70-100		GROUP 2 = 45-69		GROUP 3 = 44 or less	

Attachment 5

**HERITAGE BUILDING EVALUATION SCORE SHEET**

Municipal Address: 14452 Yonge Street  
 Legal Description: \_\_\_\_\_ Lot: \_\_\_\_\_ Cons: \_\_\_\_\_  
 Date of Evaluation: Oct 5<sup>th</sup> /16 Name of Recorder: JH Group: 2

HISTORICAL	E	G	F	P	TOTAL
Date of Construction	30	20	10	0	10/30
Trends/Patterns/Themes	40	27	14	0	27/40
Events	15	10	5	0	0/15
Persons/Groups	15	10	5	0	0/15
Archaeological (Bonus)	10	7	3	0	0/10
Historic Grouping (Bonus)	10	7	3	0	30/10
Construction Date (Bonus)	10				0/10
<b>HISTORICAL TOTAL</b>					<b>40/100</b>

ARCHITECTURAL	E	G	F	P	TOTAL
Design	20	13	7	0	20/20
Style	30	20	10	0	10/30
Architectural Integrity	20	13	7	0	13/20
Physical Condition	20	13	7	0	13/20
Design/Builder	10	7	3	0	3/10
Interior (Bonus)	10	7	3	0	7/10
<b>ARCHITECTURAL TOTAL</b>					<b>66/100</b>

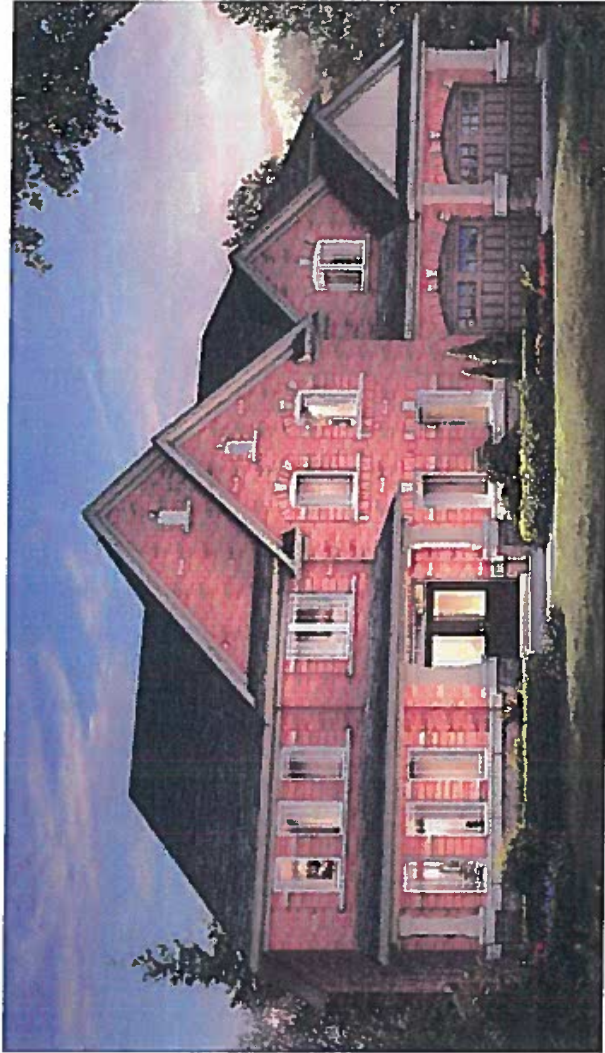
ENVIRONMENTAL	E	G	F	P	TOTAL
Design Compatibility	40	27	14	0	27/40
Community Context	20	13	7	0	0/20
Landmark	20	13	7	0	0/20
Site	20	13	7	0	20/20
<b>ENVIRONMENTAL TOTAL</b>					<b>47/100</b>

SCORE	INDIVIDUAL	OLD AURORA
Historical Score	40 X 40% = <u>16</u>	X 20% = _____
Architectural Score	66 X 40% = <u>26.4</u>	X 35% = _____
Environmental Score	47 X 20% = <u>9.4</u>	X 45% = _____
<b>TOTAL SCORE</b>	<b>51.8</b>	<input type="text"/>

GROUP 1 = 70-100                      GROUP 2 = 45-69                      GROUP 3 = 44 or less

Schedule "1"

# Bayside



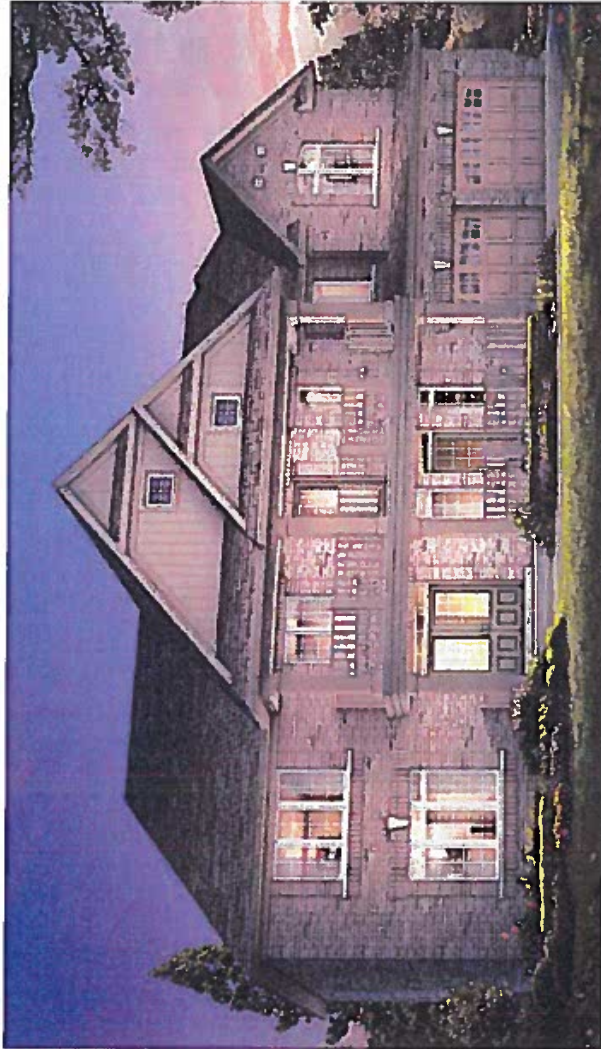
Arbit's Concept

ELEVATION A - 3,553 SQ.FT.



Schedule "I"

# Bayside



Arnett's Concept

ELEVATION B - 3,549 SQ.FT.



**Town of Aurora**

**Heritage Advisory Committee Report No. HAC18-006**

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**Subject: East Holland River, Fish Barrier Removal, Restoration and Bridge Replacement**

**Prepared by: Sara Tienkamp, Manager, Parks & Fleet**

**Department: Operational Services**

**Date: March 5, 2018**

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### **Recommendation**

- 1. That Report No. HAC18-006 be received; and**
- 2. That the Heritage Advisory Committee recommend to Council:**
  - a) THAT the Town of Aurora to enter into an agreement with Lake Simcoe Region Conservation Authority on the East Holland River, Fish Barrier Removal, Restoration and Bridge Replacement project; and**
  - b) THAT staff be authorized to proceed with the preferred option to remove and replace the concrete culvert with a steel span bridge, salvaging the culvert for display opportunities.**

### **Executive Summary**

This report seeks Council authorization to enter into an agreement with the Lake Simcoe Region Conservation Authority (LSRCA) to replace two (2) stream crossings, install bridges and complete restorative works along the East Holland River.

- LSRCA's proposal to remove fish barriers will increase habitat and range of spawning for Brook trout and other species;
- Impact of localized flooding on trail use and fish habitat reduced;
- Site two concrete box culvert constructed in early 1900's with structural integrity issues to be replaced with bridge;
- The educational and cultural heritage aspects of former sawmill founded by the Lloyd family will be promoted;
- LSRCA will fund design, and ensure funding is allocated for construction and contract administration with Town of Aurora supplying bridges.



- Timeline of works to be completed in first quarter of 2019

## **Background**

The Town's Parks Division staff was approached by LSRCA staff in February 2017 to discuss a project initiative LSRCA had for the removal of two (2) barriers along the East Holland River that would allow for fish passage and increased habitat. The project would connect an upstream tributary length of 5,500m to a downstream section of 7,400m resulting in nearly 13km of the East Holland River tributary being reconnected.

Parks Division staff had also identified both crossings for capital work funding. The upstream crossing (Site1) consists of two steel 36" culverts, insufficient in diameter to deal with flow demands during storm conditions, causing overland flooding to pedestrian path and surrounding forest. As well, the culverts are obstructed with silt and debris. The downstream crossing (Site 2) is a concrete culvert with historical association to the former saw mill founded by the Lloyd family and has been identified to have significant structural integrity concerns.

Staff recognized the value of this proposed partnership with LSRCA and the benefits this project would bring to the health of the tributary and the added experience to residents utilizing the Tim Jones and Klaus Wehrenburg trail systems. It was also acknowledged that this project would be a great opportunity to replace the insufficient, aging infrastructure.

## **Analysis**

### **LSRCA's proposal to remove fish barriers will increase habitat and range of spawning for Brook trout and other species**

LSRCA identified two (2) areas for restoration in close proximity of each other south of Sheppard's Bush Conservation Area. LSRCA has monitored the fish population along this stretch of the East Holland River and has supporting data that indicates a healthy population of Brook Trout, as well as other species such as Mottled Sculpin, Creek Chub, Fathead Minnow and Brook Stickleback. All species would significantly benefit from the project of removing the stream barriers and restoration of habitat, as it would reconnect many kilometers of the East Holland River.

### **Impact of localized flooding on trail use and fish habitat reduced**

Site 1 and Site 2 are both prone to overland flooding during heavy rain events and spring thaws. The increased flow cannot be handled by the culverts at both locations due to insufficient culvert capacity, degraded infrastructure, silt and debris. This causes severe flooding of the trail system which inhibits recreational use by residents and causing severe scouring, increasing maintenance for Parks staff in order to keep trail safe for use. As well the trail surface material migrates both into the forest and stream with the flooding affecting the woodland plants and fish habitat.

The redesign of these crossings, removal of culverts and installation of bridges will allow for greater flow of water, significantly minimize the negative effects on the woodlands and fish habitat and improve conditions for trail users.

### **Site 2 concrete box culvert constructed in early 1900's with structural integrity issues to be replaced with bridge.**

In 2015 the Town of Aurora, Engineering Department retained Safe Roads Engineering (SRE 2015) to assess 29 structures owned and operated by the Town of Aurora. Included in this assessment was the concrete box culvert (Site 2) that carries the East Holland River under the Tim Jones Trail. This report identified various deficiencies from erosion and age, emphasizing wide cracking exacerbated by vegetation growth. A recommendation was made that vehicle loading should be avoided, as the culvert was past the point of economical rehabilitation or maintenance. SRE recommended the structure be completely replaced.

As part of LSRCA's East Holland Tributary Barrier Removal and Restoration Request for Proposal, a heritage impact assessment was completed by ASI Archaeological and Cultural Heritage Services (ASI) with respect to the concrete box culvert. The report evaluated the cultural heritage significance of the East Holland River culvert and assessed the impacts of the proposed project to remove and reconstruct the crossing, while considering the cultural heritage value. The culvert was constructed in the early twentieth century as part of the former mill founded by the Lloyd Family.

It was found that based on archival research, analysis of culvert design and design in Ontario, site investigation and heritage evaluation, the culvert was determined to retain

March 5, 2018

Page 4 of 7

Report No. HAC18-006

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cultural heritage value under Regulation 9/06 of the Ontario Heritage Act, and therefore could be considered for municipal designation.

ASI presented a number of recommendations and mitigation measures for LSRCA and The Town of Aurora to take into account, seeing the cultural heritage value of the culvert. All options were given consideration but due to the structural integrity concerns with the existing culvert, associated capital costs and long term infrastructure demands, the preferred option is to remove and replace the culvert with a steel span bridge, salvaging the culvert for display opportunities adjacent to the stream.

**Natural environment improvements and cultural heritage aspects of former sawmill founded by the Lloyd family will be promoted through education and awareness**

Elements of the culvert will be salvaged and incorporated into a display along the trail adjacent to the stream, highlighting the heritage value of the culvert, the Lloyd family lands and associated saw mill operations.

There is proposed to be interpretive signage that will link the former structure both physically and functionally to its surroundings with respect to the milling practices by the Lloyd family on the property, which began in the late nineteenth century.

The salvaged components of concrete culvert are interesting in that they have exposed saw blades imbedded in the concrete, which will be showcased under a protective barrier allowing for safe viewing by the public. In some retrospect the cultural heritage of the structure will be better understood and recognized as the public cannot see these interesting features as they are contained within the culvert as it exists currently.

LSRCA and the Town plan to include community groups in the planting of vegetation as part of the restoration works, while promoting education and awareness of the natural environment improvements of increased fish habitat, spawning sites and stream rehabilitation.

The display and interpretive signage describing both the heritage value and attributes to the natural landscape will significantly add to the trail system experience of users.

**LSRCA will fund design, and ensure funding is allocated for construction and contract administration with Town of Aurora supplying bridges**

Lake Simcoe Region Conservation Authority will fund managing the design and ensure funding is allocated for construction to remove and retrofit the two (2) stream crossings along the East Holland tributary.

Town staff will review, comment and approve the work completed by LSRCA as key stakeholders in the project. The Town will also supply the two (2) bridges to replace the culverts at both Site 1 and Site 2, utilizing funding from approved capital budget for bridges. The Town, as the owner of the lands and infrastructure, will be responsible for the maintenance in perpetuity.

The Town benefits from this project by receiving retrofitted crossings that will improve the flow of water and trail conditions by reducing frequency and severity of flooding; increasing aquatic habitat; enhancing woodlands; improving aesthetics of crossings and showcasing the cultural heritage of the area.

### **Timeline of the works to be completed in first quarter of 2019**

If approved by Council, LSRCA provides following timeline for the works:

- Finalization of detail design package – 2018;
- Permits approval – March 2018 (incl. MNRF, DFO, & LSRCA)
- Tender for construction – May 2018
- Construction to commence – September 2018

Removal of crossings, in water works and bridges are to be installed during early winter of 2018-2019 as trail use is minimal and conditions ideal for construction. Anticipated timing for this work is approximately 6 weeks for both crossings. Project will be completed with vegetative planting involving community groups occur in spring 2019.

### **Advisory Committee Review**

Not applicable

### **Financial Implications**

All financial obligations associated with design, contract administration and construction will be borne by LSRCA.

March 5, 2018

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In 2015 and 2016 \$65,000 was approved for Parks staff to obtain a structural engineer to complete an assessment on the concrete culvert structure. In fall 2015, the Engineering Department retained an engineering firm to assess their infrastructure and Parks was able to utilize this firm to assess the culvert at minimal cost.

Staff request approval to utilize the remaining approved capital funds from project #73193 - Bridge Assessment, in the amount of \$64,491 to construct the two (2) steel bridges.

### **Communications Considerations**

Communications Department staff to update Town of Aurora website with construction information as necessary if project approved.

### **Legal Considerations**

N/A

### **Link to Strategic Plan**

The Award of Tender supports the Strategic Plan Goal of Supporting an Exceptional Quality of Life for All, by encouraging an active and healthy lifestyle.

Develop a long-term needs assessment for recreation programs, services and operations to match the evolving needs of the growing and changing population.

### **Alternative(s) to the Recommendation**

Council may decide to reject LSRCA's request to manage and fund the retrofit of this project with support of the Town of Aurora.

### **Conclusions**

Staff recommends that the Town of Aurora enter into an agreement with the LSRCA to replace two (2) stream crossings, install bridges and complete restorative works along the East Holland River. LSRCA will fund entirely the design, and ensure funding is allocated for construction, contract administration, site inspection and performance monitoring of the project with the Town supplying the bridge structures.

March 5, 2018

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## **Attachments**

Attachment # 1 – 17EA-089 East Holland River Barrier HIA

Attachment # 2 – Aquafor-Design Drawings-East Holland River Barrier Removal

Attachment # 3 – OSIM Final #29


## **Previous Reports**

None.

## **Pre-submission Review**

Agenda Management Meeting review on February 22, 2018

## **Departmental Approval**



**Marco Ramunno**

**Director**

**Planning and Development Services**

Attachment 1

**HERITAGE IMPACT ASSESSEMENT  
EAST HOLLAND TRIBUTARY BARRIER**

**EAST HOLLAND RIVER  
LOT 78, CONCESSION 1  
TOWN OF AURORA, ONTARIO**

Prepared for:

**Aquafor Beech Limited**  
2600 Skymark Avenue, Suite 202  
Mississauga, ON L4W 5B2

ASI File 17CH-079

December 2017



**HERITAGE IMPACT ASSESSEMENT  
EAST HOLLAND TRIBUTARY BARRIER**

**EAST HOLLAND RIVER  
LOT 78, CONCESSION 1  
TOWN OF AURORA, ONTARIO**

**EXECUTIVE SUMMARY**

ASI was contracted by Aquafor Beech Limited to conduct a Heritage Impact Assessment of the East Holland Barrier as part of the East Holland Tributary Removal and Restoration of two structures along the watercourse. The subject culvert structure carries the Tim Jones Trail over a tributary of the East Holland River in the Town of Aurora, Ontario. The East Holland Barrier culvert is currently owned by the Town of Aurora.

This report will evaluate the cultural heritage significance of the East Holland Barrier culvert and assess impacts of the proposed undertaking in consideration of its determined cultural heritage value. The subject culvert was constructed in the early twentieth century as part of the former mill founded by the Lloyd family.

Based on the results of archival research, an analysis of culvert design and construction in Ontario, field investigations, and heritage evaluation, the East Holland Barrier culvert was determined to retain cultural heritage value following application of Regulation 9/06 of the *Ontario Heritage Act*, and therefore may be considered for municipal designation.

Given the identified cultural heritage value of the East Holland River Barrier culvert, the following recommendations and mitigation measures should be considered and implemented:

1. Conservation Alternatives 1 -3 are the preferred conservation options, with Alternative 1, the retention of the structure with no major modifications undertaken, being the most preferred.
2. Should retention or relocation of the structure be chosen as the preferred conservation option (one of Conservation Alternatives 1 – 7), the heritage attributes identified in Section 5.1 should be retained and treated sympathetically.
3. Should replacement of the structure be chosen, (Conservation Alternatives 8 or 9), the following mitigation options should be considered:
  - a. Replacement/removal of the existing structure and construction of a new bridge. Where possible, salvage elements of the structure for incorporation into the new structure or for future conservation work or displays. The heritage attributes identified in Section 5.1 should be considered for replication.





- b. Compatible new development, where a new bridge is given a design that is sympathetic to the design qualities of the original bridge and its setting. This option would allow simplification of original design details and the use of new technologies and materials.
4. Should replacement of the structure be chosen, a full documentation report of the structure is required. A documentation report should be completed even if a new structure is designed to replicate the existing structure sympathetically.
5. This report should be submitted to heritage staff at the Town of Aurora, the Ministry of Transportation, and the Ministry of Tourism, Culture, and Sport for review and comment.



*Cultural Heritage Impact Assessment*  
*East Holland Tributary Barrier*  
*Town of Aurora, York Region*

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**PROJECT PERSONNEL**

<i>Senior Project Manager:</i>	Annie Veilleux, MA, CAHP <i>Senior Heritage Specialist / Manager</i> <i>Cultural Heritage Division</i>
<i>Project Manager:</i>	Joel Konrad, PhD <i>Cultural Heritage Specialist</i> <i>Cultural Heritage Division</i>
<i>Project Coordinator:</i>	Sarah Jagelewski, Hon. BA <i>Archaeologist / Assistant Manager</i> <i>Environmental Assessment Division</i>
<i>Project Administrator:</i>	Carol Bella, Hon. BA <i>Research Archaeologist</i>
<i>Archival Research:</i>	Joel Konrad
<i>Report Preparation:</i>	Joel Konrad
<i>Graphics Preparation:</i>	Adam Burwell, MSc <i>Archaeologist / Geomatics Specialist</i> <i>Operations Division</i>
<i>Report Reviewer:</i>	Annie Veilleux



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**1.0 INTRODUCTION**

ASI was contracted by Aquafor Beech Limited to conduct a Heritage Impact Assessment of the East Holland Barrier as part of the East Holland Tributary Removal and Restoration of two structures along the watercourse. The subject culvert structure carries the Tim Jones Trail over a tributary of the East Holland River in the Town of Aurora, Ontario (Figure 1). The East Holland Barrier culvert is currently owned by the Town of Aurora.

This report will evaluate the cultural heritage significance of the structure and assess impacts of the proposed undertaking in consideration of its determined cultural heritage value.

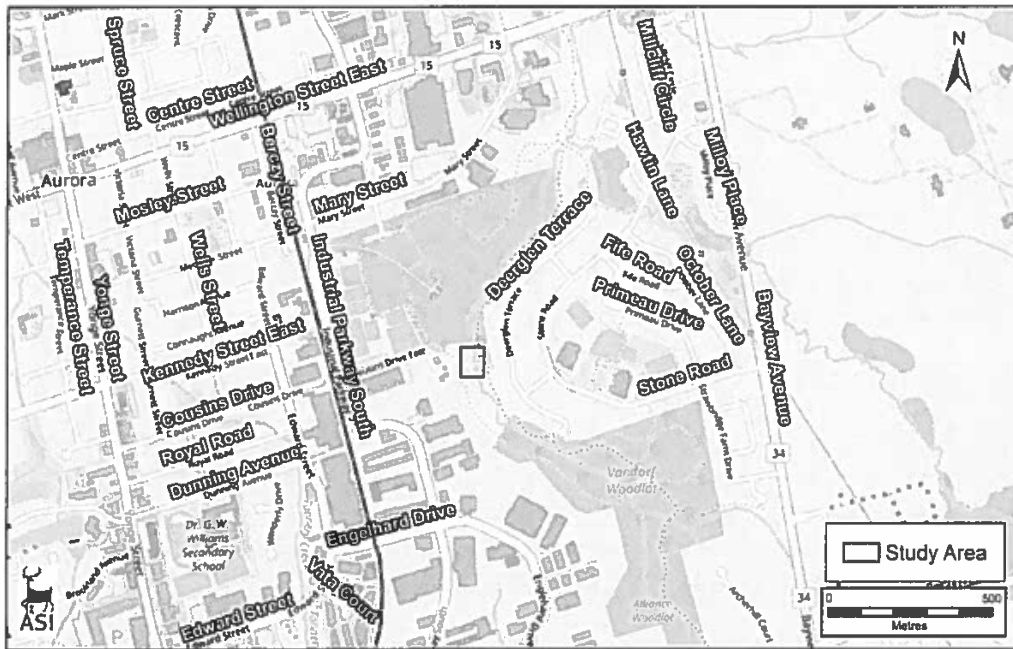


Figure 1: Location of the Study Area.

Base Map: ©OpenStreetMap and contributors, Creative Commons-Share Alike License (CC-BY-SA ESRI Street Maps)

The following report is presented as part of an approved planning and design process subject to Environmental Assessment (EA) requirements. This portion of the EA study is intended to address the proposed replacement/rehabilitation of the subject structure. The principal aims of this report are to:

- Describe the methodology that was employed and the legislative and policy context that guides heritage evaluations of structures over 40 years old;
- Provide an historical overview of the design and construction of the structure within the broader context of the surrounding township and culvert construction generally;
- Describe existing conditions and heritage integrity;



- Evaluate the culvert using Regulation 9/06 of the *Ontario Heritage Act* and draw conclusions about the heritage attributes of the structure; and
- Assess impacts of the undertaking, ascertaining sensitivity to change in the context of identified heritage attributes and recommend appropriate mitigation measures.

## **2.0 BUILT HERITAGE RESOURCE AND CULTURAL HERITAGE LANDSCAPE ASSESSMENT CONTEXT**

### **2.1 Legislation and Policy Context**

This cultural heritage assessment considers cultural heritage resources in the context of improvements to specified areas, pursuant to the *Environmental Assessment Act*. This assessment addresses above ground cultural heritage resources over 40 years old. Use of a 40-year-old threshold is a guiding principle when conducting a preliminary identification of cultural heritage resources (Ministry of Transportation 2006; Ministry of Transportation 2007; Ontario Realty Corporation 2007). While identification of a resource that is 40 years old or older does not confer outright heritage significance, this threshold provides a means to collect information about resources that may retain heritage value. Similarly, if a resource is slightly younger than 40 years old, this does not preclude the resource from retaining heritage value.

For the purposes of this assessment, the term cultural heritage resources was used to describe both cultural heritage landscapes and built heritage resources. A cultural landscape is perceived as a collection of individual built heritage resources and other related features that together form farm complexes, roadscapes and nucleated settlements. Built heritage resources are typically individual buildings or structures that may be associated with a variety of human activities, such as historical settlement and patterns of architectural development.

The analysis throughout the study process addresses cultural heritage resources under various pieces of legislation and their supporting guidelines. Under the *Environmental Assessment Act* (1990) environment is defined in Subsection 1(c) to include:

- cultural conditions that influence the life of man or a community, and;
- any building, structure, machine, or other device or thing made by man.

The Ministry of Tourism, Culture and Sport is charged under Section 2 of the *Ontario Heritage Act* with the responsibility to determine policies, priorities and programs for the conservation, protection and preservation of the heritage of Ontario and has published two guidelines to assist in assessing cultural heritage resources as part of an environmental assessment: *Guideline for Preparing the Cultural Heritage Resource Component of Environmental Assessments* (1992), and *Guidelines on the Man-Made Heritage Component of Environmental Assessments* (1981). Accordingly, both guidelines have been utilized in this assessment process.

The *Guidelines on the Man-Made Heritage Component of Environmental Assessments* (Section 1.0) states the following:

When speaking of man-made heritage we are concerned with the works of man and the effects of his activities in the environment rather than with movable human artifacts or those environments that are natural and completely undisturbed by man.



In addition, environment may be interpreted to include the combination and interrelationships of human artifacts with all other aspects of the physical environment, as well as with the social, economic and cultural conditions that influence the life of the people and communities in Ontario. The *Guidelines on the Man-Made Heritage Component of Environmental Assessments* distinguish between two basic ways of visually experiencing this heritage in the environment, namely as cultural heritage landscapes and as cultural features.

Within this document, cultural heritage landscapes are defined as the following (Section 1.0):

The use and physical appearance of the land as we see it now is a result of man's activities over time in modifying pristine landscapes for his own purposes. A cultural landscape is perceived as a collection of individual man-made features into a whole. Urban cultural landscapes are sometimes given special names such as townscapes or streetscapes that describe various scales of perception from the general scene to the particular view. Cultural landscapes in the countryside are viewed in or adjacent to natural undisturbed landscapes, or waterscapes, and include such land uses as agriculture, mining, forestry, recreation, and transportation. Like urban cultural landscapes, they too may be perceived at various scales: as a large area of homogeneous character; or as an intermediate sized area of homogeneous character or a collection of settings such as a group of farms; or as a discrete example of specific landscape character such as a single farm, or an individual village or hamlet.

A cultural feature is defined as the following (Section 1.0):

...an individual part of a cultural landscape that may be focused upon as part of a broader scene, or viewed independently. The term refers to any man-made or modified object in or on the land or underwater, such as buildings of various types, street furniture, engineering works, plantings and landscaping, archaeological sites, or a collection of such objects seen as a group because of close physical or social relationships.

The Minister of Tourism, Culture, and Sport has also published *Standards and Guidelines for Conservation of Provincial Heritage Properties* (April 2010; Standards and Guidelines hereafter). These Standards and Guidelines apply to properties the Government of Ontario owns or controls that have cultural heritage value or interest. They are mandatory for ministries and prescribed public bodies and have the authority of a Management Board or Cabinet directive. Prescribed public bodies include:

- Agricultural Research Institute of Ontario
- Hydro One Inc.
- Liquor Control Board of Ontario
- McMichael Canadian Art Collection
- Metrolinx
- The Niagara Parks Commission.
- Ontario Heritage Trust
- Ontario Infrastructure and Lands Corporation
- Ontario Lottery and Gaming Corporation
- Ontario Power Generation Inc.
- Royal Botanical Gardens
- Toronto Area Transit Operating Authority



- St. Lawrence Parks Commission

The Standards and Guidelines provide a series of definitions considered during the course of the assessment:

A provincial heritage property is defined as the following (14):

Provincial heritage property means real property, including buildings and structures on the property, that has cultural heritage value or interest and that is owned by the Crown in right of Ontario or by a prescribed public body; or that is occupied by a ministry or a prescribed public body if the terms of the occupancy agreement are such that the ministry or public body is entitled to make the alterations to the property that may be required under these heritage standards and guidelines.

A provincial heritage property of provincial significance is defined as the following (14):

Provincial heritage property that has been evaluated using the criteria found in Ontario Heritage Act O.Reg. 10/06 and has been found to have cultural heritage value or interest of provincial significance.

A built heritage resource is defined as the following (13):

...one or more significant buildings (including fixtures or equipment located in or forming part of a building), structures, earthworks, monuments, installations, or remains associated with architectural, cultural, social, political, economic, or military history and identified as being important to a community. For the purposes of these Standards and Guidelines, "structures" does not include roadways in the provincial highway network and in-use electrical or telecommunications transmission towers.

A cultural heritage landscape is defined as the following (13):

... a defined geographical area that human activity has modified and that has cultural heritage value. Such an area involves one or more groupings of individual heritage features, such as structures, spaces, archaeological sites, and natural elements, which together form a significant type of heritage form distinct from that of its constituent elements or parts. Heritage conservation districts designated under the Ontario Heritage Act, villages, parks, gardens, battlefields, mainstreets and neighbourhoods, cemeteries, trails, and industrial complexes of cultural heritage value are some examples.

Additionally, the *Planning Act* (1990) and related *Provincial Policy Statement (PPS)*, which was updated in 2014, make a number of provisions relating to heritage conservation. One of the general purposes of the *Planning Act* is to integrate matters of provincial interest in provincial and municipal planning decisions. In order to inform all those involved in planning activities of the scope of these matters of provincial interest, Section 2 of the *Planning Act* provides an extensive listing. These matters of provincial interest shall be regarded when certain authorities, including the council of a municipality, carry out their responsibilities under the *Act*. One of these provincial interests is directly concerned with:

- 2.(d) the conservation of features of significant architectural, cultural, historical, archaeological or scientific interest



Part 4.7 of the *PPS* states that:

The official plan is the most important vehicle for implementation of this Provincial Policy Statement. Comprehensive, integrated and long-term planning is best achieved through official plans.

Official plans shall identify provincial interests and set out appropriate land use designations and policies. To determine the significance of some natural heritage features and other resources, evaluation may be required.

Official plans should also coordinate cross-boundary matters to complement the actions of other planning authorities and promote mutually beneficial solutions. Official plans shall provide clear, reasonable and attainable policies to protect provincial interests and direct development to suitable areas.

In order to protect provincial interests, planning authorities shall keep their official plans up-to-date with this Provincial Policy Statement. The policies of this Provincial Policy Statement continue to apply after adoption and approval of an official plan.

Those policies of particular relevance for the conservation of heritage features are contained in Section 2-Wise Use and Management of Resources, wherein Subsection 2.6 - Cultural Heritage and Archaeological Resources, makes the following provisions:

- 2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved.

A number of definitions that have specific meanings for use in a policy context accompany the policy statement. These definitions include built heritage resources and cultural heritage landscapes.

A *built heritage resource* is defined as: “a building, structure, monument, installation or any manufactured remnant that contributes to a property’s cultural heritage value or interest as identified by a community, including an Aboriginal community” (PPS 2014).

A *cultural heritage landscape* is defined as “a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Aboriginal community. The area may involve features such as structures, spaces, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association” (PPS 2014). Examples may include, but are not limited to farmscapes, historic settlements, parks, gardens, battlefields, mainstreets and neighbourhoods, cemeteries, trailways, and industrial complexes of cultural heritage value.

In addition, significance is also more generally defined. It is assigned a specific meaning according to the subject matter or policy context, such as wetlands or ecologically important areas. With regard to cultural heritage and archaeology resources, resources of significance are those that are valued for the important contribution they make to our understanding of the history of a place, an event, or a people (PPS 2014).

Criteria for determining significance for the resources are recommended by the Province, but municipal approaches that achieve or exceed the same objective may also be used. While some significant resources





may already be identified and inventoried by official sources, the significance of others can only be determined after evaluation (PPS 2014).

Accordingly, the foregoing guidelines and relevant policy statement were used to guide the scope and methodology of the cultural heritage assessment.

## **2.2 Municipal Policies**

The Town of Aurora has developed an Official Plan (2010, Section 13: Conserving Cultural Heritage Resources), which sets out a number of policies with regard to cultural heritage resources. General policies that are relevant to this study include the following:

- 13.0 Preserving heritage enhances the diversity, beauty and richness of the natural and built environments. Rapid social and economic change tends to cause human stress. The presence of heritage helps sustain a sense of perspective and identity.
- 13.1 Objectives
- Conserve and enhance recognized cultural heritage resources of the Town for the enjoyment of existing and future generations;
  - Preserve, restore and rehabilitate structures, buildings or sites deemed to have significant historic, archaeological, architectural or cultural significance and, preserve cultural heritage landscapes; including significant public views; and;
  - Promote public awareness of Aurora's cultural heritage and involve the public in heritage resource decisions affecting the municipality.
- 13.2 General Cultural Heritage Policies
- Heritage planning is the joint responsibility of the Provincial Government, the Region and the Town. An Advisory Committee, known as the Aurora Heritage Advisory Committee has been established to provide advice to the Town Council on all matters pertaining to heritage.
  - The Town may use the power and tools provided by the enabling legislation, policies and programs, particularly the Ontario Heritage Act, The Planning Act, the Environmental Assessment Act and the Municipal Act in implementing and enforcing the policies of the section. These may include but not be limited to the following:
    - The power to stop demolition and/or alteration of designated heritage properties and resources provided under the Ontario Heritage Act and as set out in Section 13.3 of this policy;
    - The power to require a Heritage Impact Assessment and Restoration/Conservation Plan for development proposals and other land use planning proposals that may potentially affect a designated or significant heritage resource or Heritage Conservation District;
    - Using zoning by-law provisions to protect heritage resources by regulating such matters as use, massing, form, design, location, and setbacks;
    - Using the site plan control by-law to ensure that new development is compatible with heritage resources ;
    - Using parkland dedication requirements to conserve significant heritage resources;



- Identifying, documenting and designating cultural heritage resources as appropriate in the secondary and block plans and including measures to protect and enhance any significant heritage resources identified as part of the approval conditions; and
- Using fiscal tools and incentives to facilitate heritage conservation including but not limited to the Community Improvement Plan and Façade Improvement Program pursuant to the Planning Act, grants and loans pursuant to the Planning Act, grants and loans pursuant to the Ontario Heritage Act, and heritage property tax reduction/rebate program pursuant to the Municipal Act.
- The Town's by-laws, regulations and standards shall be sensitive to the Town's heritage resources and may permit non-standard solutions in order to support the Town's objectives for heritage preservation. Specific measures may include, but are not limited to reduced lot sizes, reduced setbacks and alternative parking requirements.

In addition, the Town of Aurora has created a *Heritage Impact Assessments and Conservation Plans Guide* (2016) which also informs this report. As outlined in the Town's *Guide*, this report employs *Ontario Heritage Act* Regulation 9/06 to evaluate the subject resource and provides the information required as per section 5.3 of the *Guide*.

### **2.2.1 Municipal Consultation**

The Town of Aurora was also consulted for additional information on the subject culvert via email (12 December 2017). As of the writing of this report no response was received. In addition, email and phone communication with Brook Piatrowski, Urban Restoration Construction Specialist at the Lake Simcoe Region Conservation Authority, was also conducted (5 December 2017). No additional information was attained at that time.

### **2.3 Cultural Heritage Evaluation and Heritage Impact Assessment Report**

The scope of a Cultural Heritage Evaluation (CHE) is guided by the Ministry of Tourism, Culture and Sport's *Ontario Heritage Toolkit* (2006). Generally, CHEs include the following components:

- A general description of the history of the study area as well as a detailed historical summary of property ownership and building(s) development;
- A description of the cultural heritage landscape and built heritage resources;
- Representative photographs of the exterior and interior of a building or structure, and character-defining architectural details;
- A cultural heritage resource evaluation guided by the *Ontario Heritage Act* criteria;
- A summary of heritage attributes;
- Historical mapping, photographs; and
- A location plan.

Using background information and data collected during the site visit, the cultural heritage resource is evaluated using criteria contained within Regulation 9/06 of the *Ontario Heritage Act*.



Ontario Heritage Act Regulation 9/06 provides a set of criteria, grouped into the following categories which determine the cultural heritage value or interest of a potential heritage resource in a municipality:

- i) Design/Physical Value;
- ii) Historical/Associative Value; and
- iii) Contextual Value.

Should the potential heritage resource meet one or more of the above mentioned criteria, a Heritage Impact Assessment (HIA) is required and the resource considered for designation under the *Ontario Heritage Act*.

### **3.0 HISTORICAL CONTEXT AND CONSTRUCTION**

#### **3.1 Introduction**

The East Holland Barrier culvert is a “flat-top” concrete culvert constructed in the early twentieth century, and carries the Tim Jones Trail across a tributary of the East Holland River in the Town of Aurora, Ontario. Historically, the study area is located in Lot 78, Concession 1 in the Township of Whitchurch, York County.

Cultural heritage resources are those buildings or structures that have one or more heritage attributes. Heritage attributes are constituted by and linked to historical associations, architectural or engineering qualities and contextual values. Inevitably many, if not all, heritage resources are inherently tied to “place”; geographical space, within which they are uniquely linked to local themes of historical activity and from which many of their heritage attributes are directly distinguished today. In certain cases, however, heritage features may also be viewed within a much broader context. The following section of this report details a brief historical background to the settlement of the surrounding area. A description is also provided of the construction of the culvert within its historical context.

#### **3.2 Local History and Settlement**

##### **3.2.1 Township of Whitchurch**

The Township of Whitchurch was originally surveyed by John Stegman in 1800. Stegman surveyed from the first to fourth Concessions, with the Township survey completed in 1802 (Miles & Co. 1878). The Township was named in honour of the village of Whitchurch, Herefordshire in England, where Elizabeth Simcoe, wife of Upper Canada Lieutenant Governor Sir John Graves Simcoe, was born, and included the area bounded by present day Stouffville Road to the south, Yonge Street to the west, Davis Drive to the north and Durham Regional Road 30 to the east. The original concession and side roads first laid out by John Stegman still remain today: Woodbine, Warden, Kennedy, McCowan, Highway 48, Ninth Line, Tenth Line and Regional Road 30 were the concession roads, and Stouffville Road, Bethesda, Bloomington, Vandorf, Aurora, St John's, Vivian and Davis Drive were the side roads.

Within the Township of Whitchurch, several villages of varying sizes had developed by the end of the nineteenth century, including Newmarket, Aurora, and Stouffville. A number of crossroad communities also began to grow by the end of the nineteenth century. These included Bethesda, Lemonville, Bloomington, White Rose, Petchville, Ballantrae, Vivian, and Pine Orchard.



Many of the early settlers in Whitchurch were United Empire Loyalists, hired by the British to help fight in the American War of Independence, and Quakers, who were lured to the area with the promise of land grants and also the ability to practice their faith in peace.

### **3.2.2 Town of Aurora**

Aurora is located on Yonge Street in York County. Part of the town is in King Township and the other part is in Whitchurch. The town was formerly known as Machell's Corners, and was incorporated as a village in 1863 (Miles & Co. 1878).

The town was known as Machell's Corners because the merchant Richard Machell acquired the corner at Yonge and Wellington in 1804. A settlement developed in the area and in 1806 when a small Quaker population established a school with Timothy Rogers Jr. as a teacher. The first trains arrived in Aurora in 1853 and by 1860 the village had grown to a population of 700. The early industries included the Fleury Plow Works, a gristmill, ashery, tannery, cooperage, brewery, and chair factory. By 1888, Aurora's population was 2,107 (Mika and Mika 1977:110-111).

By the time of the arrival of the railway many of the large lots had already been subdivided in order to provide small building plots. This was especially true for Lot 80, "in both King and Whitchurch, immediately south of Wellington Street" (Aurora Heritage Committee 1984:3).

### **3.3 History of the Study Area**

Historically, the study area is located in Lot 78, Concession 1 in the Township of Whitchurch, York County. A review of historic mapping, municipal records, assessment records, and census data identifies that a sawmill existed on the property dating back to 1860, though the use of concrete in the existing structure suggests it was constructed sometime in the early twentieth century.

George Tremaine's *Map of the County of York* (Figure 2) identifies Jason Lloyd as the owner of Lot 78, Concession 1. A saw mill is identified in the northwest portion of the property in the location of the existing saw mill dam. A mill pond is located to the south of the structure with the East Holland River tributary running through the study area. An informal road is identified running through the study area and likely crossing the dam structure.

The property's owner, Jason Lloyd, is not identified in census data from the period. However, William G. Lloyd, identified as the property's owner on the 1878 *Illustrated Historical Atlas of York County* (Figure 3), is recorded in the 1861 Census Returns for the First Concession of Whitchurch Township as a 36-year-old farmer married to Sarah Lloyd (aged 28) and five children: Elizabeth Lloyd (aged 12), John Lloyd (aged 7), James Lloyd (aged 4), William Lloyd (aged 3) and Emma Lloyd (aged 1). The family lived on the property in a one-and-a-half storey frame house.

The 1881 Census Returns for Whitchurch (Sub District 8, page 23) records W.G. Lloyd as a 58 year-old farmer living on the property with his wife, Sarah (aged 48) and their eight children. In addition to being a farmer, W.G. Lloyd was a founding member of the new Town Council established in 1888, though he did not hold a position of reeve or deputy reeve (Johnston 1963: 50).



Topographic mapping from 1914 (Figure 4) indicates that the study area remained generally undeveloped, with no residences or other buildings identified immediately adjacent. In addition, no road is recorded crossing the East Holland River tributary within the vicinity of the study area. The map also reveals the associated sawmill was either not identified or had ceased to exist by the early twentieth century. As the technology and procedure for creating accurate topographic maps were in their infancy at this time, it is possible that a saw mill identified to the northeast of the study area was identified incorrectly and was actually located closer to, and associated with, the mill dam. However, no saw mill is identified in the 1943 topographic mapping (Figure 5), indicating that milling in the immediate area had been abandoned sometime prior to the mid-twentieth century. Topographic mapping for 1994 (Figure 6) indicates that little development had occurred in the intervening years between 1943 and 1994.



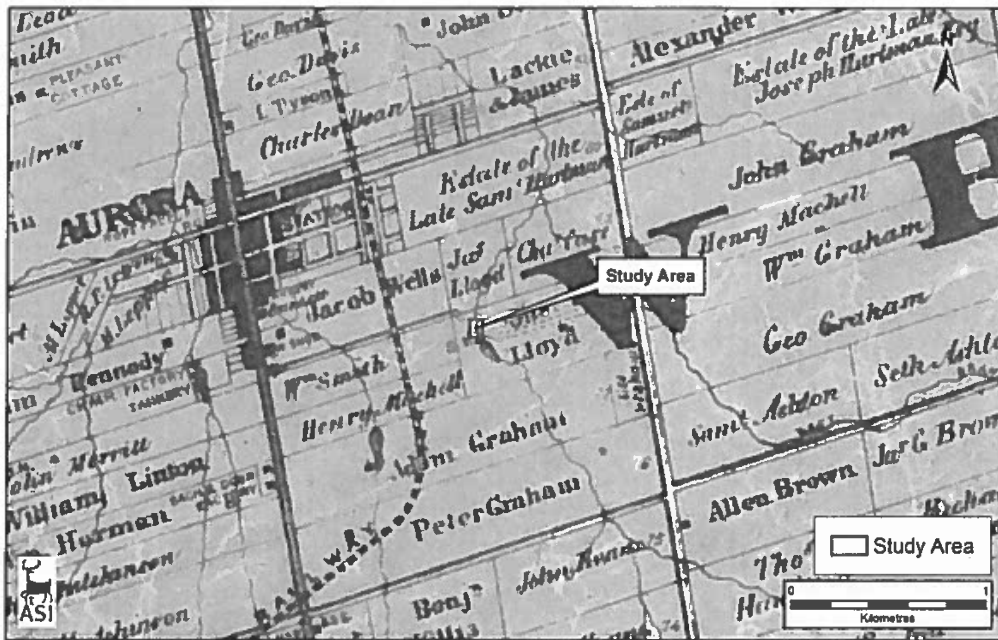


Figure 2: The study area overlaid on 1860 historical mapping  
Base Map: Tremaine's Map of the County of York, 1860

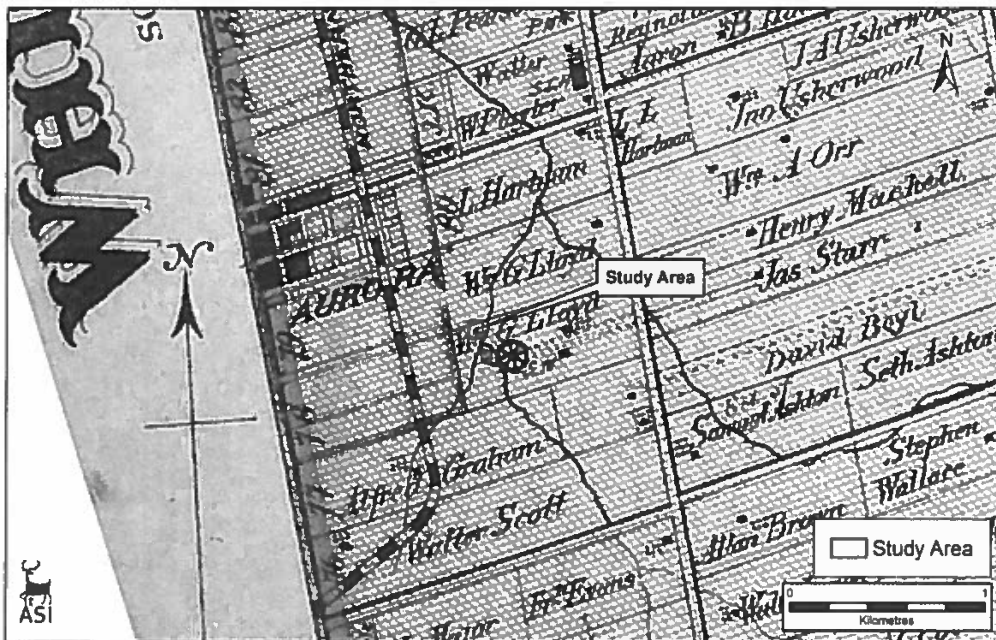


Figure 3: The study area overlaid on 1878 historical mapping  
Base Map: Miles and Co. 1878



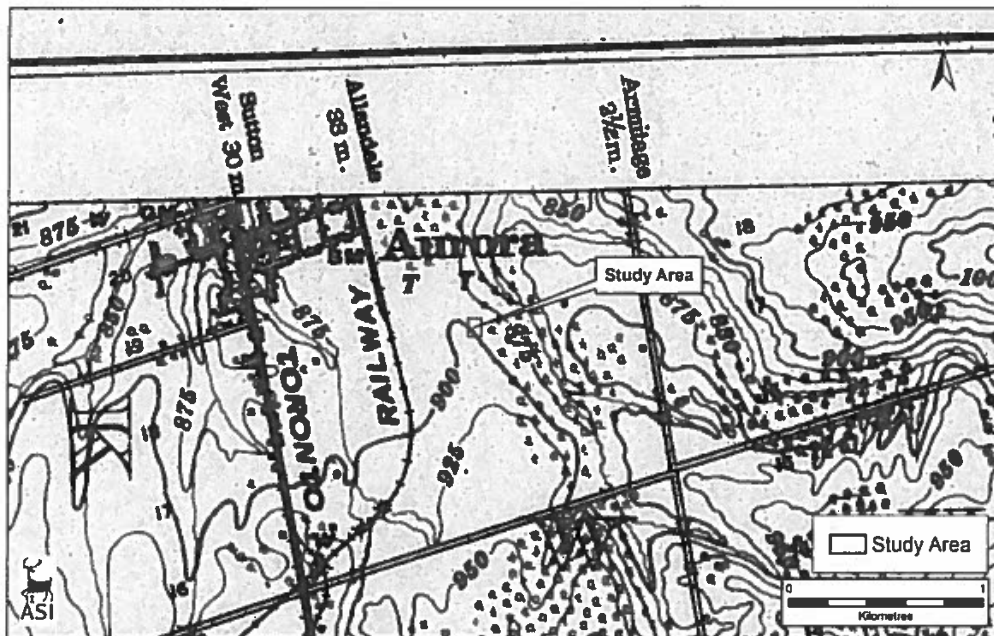


Figure 4: The study area overlaid on the 1914 NTS map  
Base Map: NTS Sheet 30M-14 (Markham) (Department of National Defense 1914)

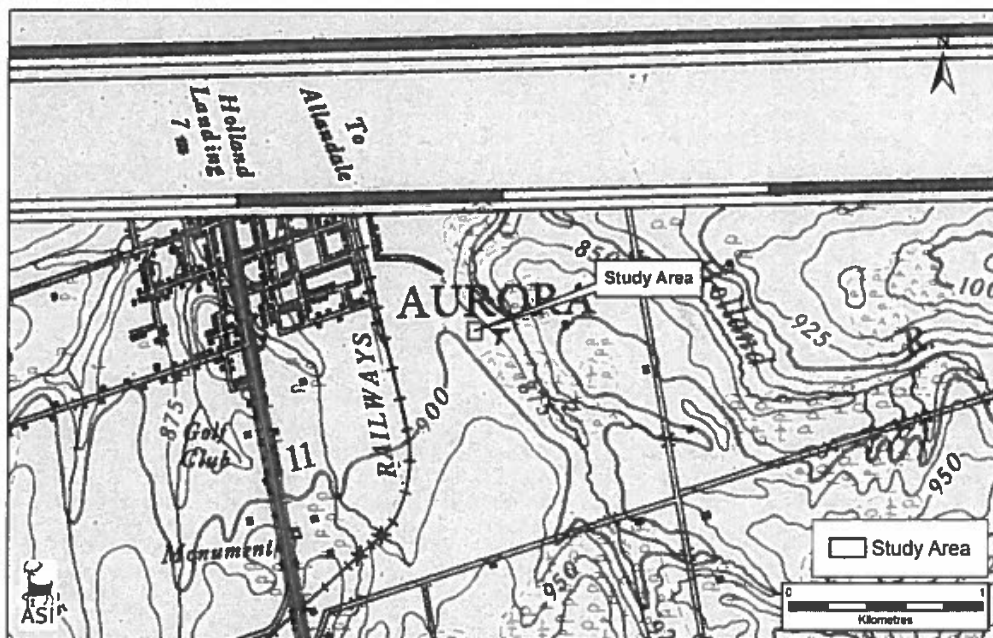


Figure 5: The study area overlaid on the 1943 NTS Map  
Base Map: NTS Sheet 31M-14 (Markham)(Department of National Defense 1943)



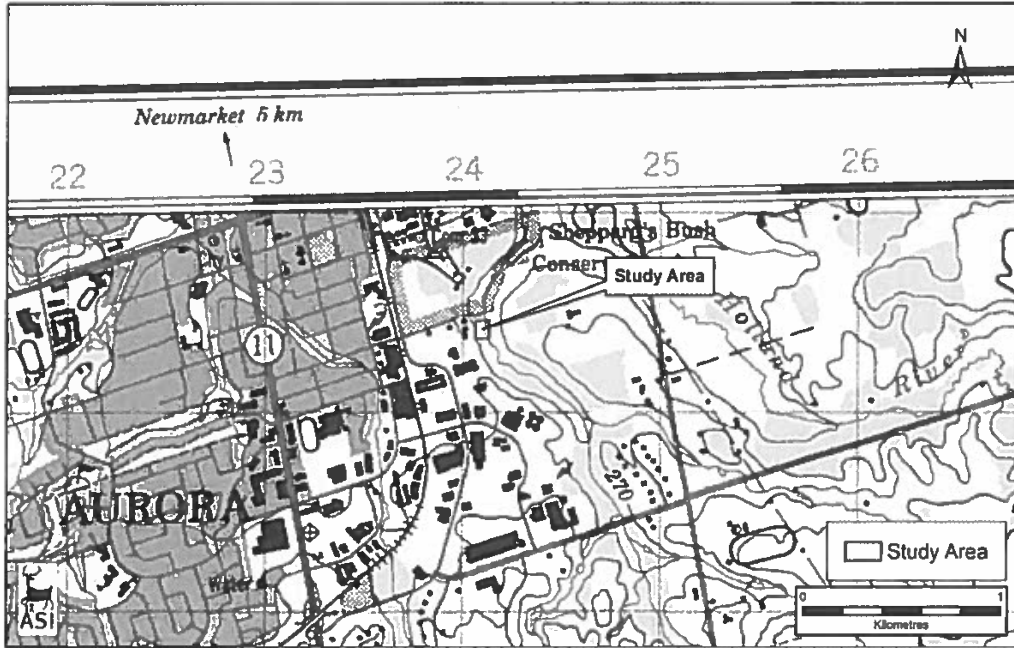


Figure 6: The study area overlaid on the 1994 NTS Map  
Base Map: NTS Sheet 31M-14 (Markham)(Department of Energy, Mines, and Resources 1994)





### **3.4 Bridge and Culvert Construction**

#### *3.4.1 Early Bridge and Culvert Building in Ontario*

Up until the 1890s, timber truss bridges were the most common bridge type built in southern Ontario. Stone and wrought iron materials were also employed but due to higher costs and a lack of skilled craftsmen, these structures were generally restricted to market towns. By the 1890s, steel was becoming the material of choice when constructing bridges given that concrete was less expensive and more durable than its wood and wrought iron predecessors. Steel truss structures were very common by 1900, as were steel girder bridges. The use of concrete in constructing bridges was introduced at the beginning of the twentieth century, and by the 1930s, it was challenging steel as the primary bridge construction material in Ontario (Ministry of Culture and Ministry of Transportation [n.d.]:7-8).

Wooden structures were generally used for short span bridges and culverts, due to the relative ease of reconstruction and the low costs associated. According to the 1899 Annual Report, cedar was principally used for culvert construction, however, even with the rot-resistant properties the cedar structures were subject to warping, frost displacement, and decay making them structurally unsound generally after eight years (Ontario Department of Public Works 1899:38). Beginning in the late nineteenth-century, these simple wooden box-culverts and sluices were replaced with more durable concrete pipe, arch, and box culverts as supplies of inexpensive quality lumber dwindled, and population growth caused increased traffic on roadways (Ontario Department of Public Works 1899). By the early twentieth-century, wooden culverts were largely replaced by more durable cast-in-place concrete structures. These cast-in-place concrete culverts were in turn increasingly replaced with precast concrete culverts in the late twentieth century due to the ease of installation, low cost, and minimal site disturbance (Stelsel 2014).

#### *3.4.2 Construction of the East Holland Barrier*

The original structural drawings for the East Holland Barrier are not available and were therefore not reviewed as part of this assessment.

Archival research, field review and the knowledge of extant concrete culverts in Ontario suggest that the structure was constructed in the early twentieth century. While no direct information is available on the construction of this structure, the Annual Report from the Commissioner of Highways, Ontario, for the year 1902 provides a description of the utility of a “concrete culvert with flat top,” which generally describes the structural type of the subject culvert (Commissioner of Highways 1903: 91). The description states that this form of structure was “cheap but durable,” however warns that displacement of the side walls was possible due to frost or uneven settlement from any cause. The following gives a description of the method of construction for these types of culverts:

The side walls are to be erected within a substantial and well-constructed framework of well-fitted lumber, closely boarded up against the work as it proceeds. Care shall be taken to make a smooth regular surface, such that the moisture will not find lodgement. The concrete shall be perfectly rammed into place so that all surfaces shall be smooth, without cavities, when the casing is removed. The framework shall not be removed in less than ten days from the completion of the work



The plank used at the top of the framework for the sidewalls shall have stout nails driven into it, two inches apart, but projecting above the top of the abutments...

A temporary framework shall be erected to support the concrete flooring while in process of construction. This framework shall be firm and substantial, of dressed lumber, in all respects to be approved by the superintendent in charge of the work. Upon this framework shall first be spread a sufficient layer of fine or cinder concrete to cover the wire. By means of a suitable hook, and while the concrete is plastic, each wire above described shall be raised above the concrete, permitting the concrete to freely surround the wire. More concrete shall be added, and the whole firmly tamped and rammed to thoroughly compact the bed of concrete, to the depth shown upon the plans attached.

All cement employed in the work must be of a favorably known brand of Portland cement, and approved by the superintendent in charge of the work. It shall be delivered in barrels or equally tight receptacles, and after delivery must be protected from the weather by storing in a tight building or by suitable covering. The packages shall not be laid directly on the ground, but shall be placed on boards raised a few inches from it.

The concrete used in the side and wing walls shall be composed of gravel and Portland cement, mixed in the proportion of one part by measure of cement to six parts of gravel, that used in the cover stone to be in the proportion of one and one-half inches in diameter. The concrete shall be mixed on a platform placed close to the work by first spreading evenly a layer of gravel, upon this shall be spread a proportionate quantity of cement, and the two thoroughly intermixed in a dry state. To this, sufficient clean water shall be slowly added, and the whole again thoroughly mixed and brought to the consistency of a stiff mortar (Commissioner of Highways 1903: 92-93).

#### **4.0 EXISTING CONDITIONS AND INTEGRITY**

A field review was undertaken by Joel Konrad on 21 November 2017 to conduct photographic documentation of the crossing and to collect data relevant for completing a heritage evaluation of the structure. Results of the field review and secondary documentation received from the client were then utilized to describe the existing conditions of the culvert crossing. This section provides a general description of the bridge crossing and associated cultural heritage features. Photographic documentation of the crossing (Plates 1-12) is provided in Appendix A.

The subject structure is identified variously as a barrier, saw mill dam, and culvert in existing literature provided by the Town of Aurora and the Lake Simcoe Region Conservation Authority. As the structure conforms to the description of a culvert according to the Ontario Structural Inspection Manual (OSIM – 2012), it will be identified as a culvert for the purposes of existing conditions description, comparative analysis, and heritage evaluation.

Historically, the structure is located within Lot 78, Concession 1 in the Township of Whitchurch, York County. The subject culvert carries a tributary of the East Holland River under the Tim Jones Trail south



of the Sheppard's Bush Conservation Area, west of Deerglen Terrace, and east of Cousins Drive East. The trail surface is generally characterized by loose-pack gravel placed over a concrete slab; wood handrails offer barrier protection on either side of the structure. The culvert's substructure is comprised of a horizontal concrete slab supported by two vertical concrete walls. Formwork lines are still visible on the concrete walls, though heavy erosion has deteriorated the ends of the structure, particularly at the north side. The south side retains wide concrete wing-walls that act as a dam for a small pool as part of the East Holland River tributary.

Generally, the landscape is marked by wooded areas transected by maintained gravel paths. A pool is located downstream from the structure with a minor pool located to the south, or upstream, of the culvert. A concrete barrier structure in a severe state of disrepair is located approximately 50 metres to the south of the culvert. It is likely this concrete structure was used as a dam to create a now removed mill pond stretching north to the culvert.

According to the Ontario Structure Inspection Manual (OSIM) report, undertaken on 25 July 2015, the culvert structure has a length of 5.5 metres, a width of 0.55 metres, and a height of 1.5 metres. While no other information on the construction or maintenance of the structure was identified, the following deficiencies were also noted:

- **Abutments:** Wide cracks on south east interior side and north east exterior side, medium scaling throughout, very severe spalling on the exterior faces, two wing walls on north side are destroyed and laying on the ground in the stream, two standing both have wide cracking;
- **Deck:** Walking path is in good to fair condition, slab soffit has severe delamination and tree growth through the soffit;
- **Barriers:** Good to fair condition.

An additional structural report was completed by Safe Roads Engineering (SRE 2015) as part of the assessment of 29 structures owned and maintained by the Town of Aurora. This report identified immediate structural needs for some of these structures, including the subject culvert. As part of this report, various deficiencies were identified, generally reflecting the OSIM report for the structure. The report emphasized the wide cracking exacerbated by vegetation, and specifically mature trees, growing within the cracks. A recommendation was made that vehicle loading be avoided as the structure is past the point of economical rehabilitation or maintenance. As such, SRE suggested that the structure be completely replaced (SRE 2015).



**4.1 Comparative Geographic and Historic Context of Concrete Box Culverts**

The subject structure is a short-span “flat-top” culvert likely constructed in the early twentieth century. Unfortunately, no inventory of culverts maintained by the municipality was available for a comparison of similar structures in the local context. As the MTO structural inventory does not include culverts, there was also no possibility of completing a comparative analysis with provincially owned structures of similar construction. However, the structure generally follows the construction guidelines for “flat-top” culverts in the Annual Report of the Commissioner of Highways (Commissioner of Highways 1903: 91), and is therefor considered of common construction for its design type. When analyzed within the range prescribed by the design directions, which identify culverts of up to three spans and states that large spans are those of “up to six or eight feet” (Commissioner of Highways 1903: 91), the subject culvert is not considered to be significant in terms of length or number of spans.

**4.2 Additional Cultural Heritage Resources**

While there are no previously identified cultural heritage resources located directly adjacent to the subject culvert, the Sheppard Estate House and Grounds, known as Brooklands, is located to the north of the study area. The estate is designated under Part IV of the *Ontario Heritage Act*, under municipal By-law number 4976.07.D.

**5.0 HERITAGE EVALUATION OF THE SUBJECT CULVERT**

Table 1 contains the evaluation of the subject culvert against criteria as set out in Regulation 9/06 of the *Ontario Heritage Act*. Within the Municipal EA process, Regulation 9/06 is the prevailing evaluation tool when determining if a heritage resource, in this case a culvert, has cultural heritage value.

**Table 1: Evaluation of the East Holland Barrier Culvert using Ontario Regulation 9/06**

1. The property has design value or physical value because it:

<i>Ontario Heritage Act</i> Criteria	Analysis
i. is a rare, unique, representative or early example of a style, type, expression, material or construction method;	The subject does not meet this criterion when analyzed within the MTO’s guidelines for the construction of “flat-top” culverts in the province of Ontario.
ii. displays a high degree of craftsmanship or artistic merit, or;	As the plans for the subject culvert were not available, the designer of the structure is unknown. However, the concrete structure generally follows the MTO’s guidelines for the construction of “flat-top” culverts in the province of Ontario and therefore does not display a high degree of craftsmanship or artistic merit.
iii. demonstrates a high degree of technical or scientific achievement.	Following review of available material, it was determined that this structure does not demonstrate a high degree of technical or scientific achievement.

2. The property has historical value or associative value because it:



<i>Ontario Heritage Act Criteria</i>	<i>Analysis</i>
i. has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community;	The culvert is historically associated with the mill run by the Lloyd family in the late nineteenth and early twentieth centuries. However, the structure itself is not an original structure within the context of milling in the area and is likely the last of a series of culvert barriers located within the area. Therefore, the subject culvert does not meet this criterion.
ii. yields, or has the potential to yield, information that contributes to an understanding of a community or culture, or;	This criterion is not satisfied given that the structure does not contribute to an understanding of a community or culture.
iii. demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.	The subject culvert's designer is unknown and is thus not known to hold any significance to the local community. The subject culvert does not meet this criterion.

3. The property has contextual value because it:

<i>Ontario Heritage Act Criteria</i>	<i>Analysis</i>
i. is important in defining, maintaining or supporting the character of an area;	The subject culvert is small in scale but does have significant visibility to pedestrians from the public right of way. In addition, the crossing helps to define the character of the area. However, it is determined that the crossing, and not the structure, is the element that defines the area's character. As such, the subject structure does not meet this criterion.
ii. is physically, functionally, visually or historically linked to its surroundings, or;	The subject culvert retains physical and visual links to the East Holland River tributary upon which it is situated. Therefore, the subject structure meets this criterion.
iii. is a landmark.	Visible to pedestrians using the Tim Jones Trail, the subject culvert is considered a defining element to the setting and a waypoint along the trail, and thus meets this criterion.

Based on available information, the East Holland Barrier culvert meets at least one of the criteria set out in Ontario Regulation 9/06. Accordingly, this structure is considered to retain cultural heritage value and may be considered for municipal designation under the *Ontario Heritage Act*.

## 5.1 Draft Statement of Cultural Heritage Value

### 5.1.1 Description of Property



The East Holland Barrier culvert is located on the Tim Jones Trail in the Town of Aurora, Ontario. The structure is a single-span “flat-top” culvert constructed of concrete that carries a pedestrian trail over a tributary of the East Holland River in a generally east-west direction.

### **5.1.2 Cultural Heritage Value**

The East Holland Barrier culvert is an example of a “flat-top” culvert. The structure was constructed in the early twentieth century, measures approximately 5.5 metres in length, and is not known to have undergone any structural modifications. Concrete “flat-top” culverts were a recommended structural category by the MTO in the early twentieth century and the East Holland Barrier culvert proves a representation of the type.

The East Holland Barrier culvert retains associations with milling practices on the property, which began with the Lloyd family in the late-nineteenth century. In addition, the structure is physically and functionally linked to its surroundings, and particularly the East Holland River tributary and the Tim Jones Trail.

### **5.1.3 List of Heritage Attributes**

A list of heritage attributes that contribute to the cultural heritage value of the East Holland Barrier culvert include:

- Location on the East Holland River tributary.

## **6.0 ALTERNATIVES TO BE CONSIDERED FOR HERITAGE BRIDGES AS PART OF THE ENVIRONMENTAL ASSESSMENT PROCESS**

Following the evaluation of the East Holland Barrier culvert it is determined that it retains cultural heritage value. The following eight conservation options/alternatives are arranged according to the level or degree of intervention from minimum to maximum. The following conservation options have been adapted from the *Ontario Heritage Bridge Program* (MCC 1991) which is regarded as current best practice for conserving heritage bridges in Ontario and ensures that heritage concerns, and appropriate mitigation options are considered. It has been applied to the subject structure as it carries pedestrian traffic over the East Holland River tributary.

1. Retention of existing structure with no major modifications undertaken;
2. Retention of existing structure and restoration of missing or deteriorated elements where physical or documentary evidence (e.g., photographs or drawings) can be used for their design;
3. Retention of existing structure with sympathetic modification;
4. Retention of existing structure with sympathetically designed new structure in proximity;



5. Retention of existing structure no longer in use for vehicle purposes but adapted for pedestrian walkways, cycle paths, scenic viewing etc.;
6. Retention of structure as heritage monument for viewing purposes only;
7. Relocation of structure to appropriate new site for continued use or adaptive re-use;
8. Replacement/removal of existing structure with salvage elements/members of heritage bridge for incorporation into new structure or for future conservation work or displays;
9. Replacement/removal of existing structure with full recording and documentation of the heritage bridge.

Given that the bridge was found to retain cultural heritage value under Regulation 9/06, all nine of these conservation options should be considered as part of the East Holland Barrier culvert Heritage Impact Assessment Report.

## **7.0 ENVIRONMENTAL ASSESSMENT OPTIONS**

The proposed development (Appendix B) will remove the current concrete culvert structure and replace it with a modern truss structure connecting the east and west sections of the Tim Jones Trail. The replacement will result in a regarding of the Tim Jones trail on the east and west banks of the East Holland River tributary and open the flow of the East Holland River Tributary through the area. The increased flow of water will result in a reduction of the small pond to the north of the culvert. The existing plan also provides for a fenced space on the west bank to be used for interpretive signage describing the heritage value and attributes of the landscape.

### **7.1 Evaluation of Impacts**

To assess the potential impacts of the proposed alternatives, the cultural heritage resource and identified heritage attributes were considered against a range of possible impacts (Table 3) as outlined in the Ministry of Tourism and Culture document entitled *Screening for Impacts to Built Heritage and Cultural Heritage Landscapes* (November 2010), which include:

- Destruction of any, or part of any, significant heritage attribute or feature (III.1).
- Alteration which means a change in any manner and includes restoration, renovation, repair or disturbance (III.2).
- Shadows created that alter the appearance of a heritage attribute or change the visibility of a natural feature of plantings, such as a garden (III.3).
- Isolation of a heritage attribute from its surrounding environment, context, or a significant relationship (III.4).
- Direct or indirect obstruction of significant views or vistas from, within, or to a built and natural feature (III.5).
- A change in land use such as rezoning a battlefield from open space to residential use, allowing new development or site alteration to fill in the formerly open spaces (III.6).
- Soil disturbance such as a change in grade, or an alteration of the drainage pattern, or excavation, etc. (III.7)



**Table 2: Evaluation of the Potential Impacts of Bridge Improvement Alternatives on the Cultural Heritage Resource and Identified Heritage Attributes**

Nine Bridge Improvement Alternatives	Destruction, removal or relocation	Alteration	Shadows	Isolation	Direct or indirect obstruction of significant views	A change in land use	Soil disturbance
1) Retention of existing structure with no major modifications undertaken	No impact.	No impact.	No impact.	No impact.	No impact.	No impact.	No impact.
2) Retention of existing structure and restoration of missing or deteriorated elements where physical or documentary evidence (e.g. photographs or drawings) can be used for their design	No impact.	No impact.	No impact.	No impact.	No impact.	No impact.	No impact.
3) Retention of existing structure with sympathetic modification	No impact.	Potential impacts to existing structure as extreme intervention would be required for rehabilitation.	No impact.	No impact.	No impact.	No impact.	No impact.
4) Retention of existing structure with sympathetically designed new structure in proximity	No impact.	Yes – impacts are expected given that a new bridge/culvert in proximity to the existing one will alter the use, immediate setting and context of the site.	No impact.	No impact.	No impact.	Yes – use of the structure would change as it would no longer be used for its current purpose as a bridging point along the East Holland River tributary.	Yes – impacts are expected through the construction of a new structure in proximity.
5) Retention of existing structure no longer in use for vehicle purposes but adapted for pedestrian walkways, cycle paths, scenic viewing etc	No impact.	Yes – a change in use would result in alterations to the heritage resource.	No impact.	No impact.	No impact.	No impact.	No impact.
6) Retention of structure as heritage monument for viewing purposes only	No impact.	Yes – use of the structure for viewing purposes only would result in a change from the original use of the structure and thus would be considered to be an alteration.	No impact.	No impact.	No impact.	Yes – use of structure for viewing purposes only would result in a change from the original use of the structure.	No impact.
7) Relocation of structure to appropriate new site for continued use or adaptive re-use	Yes – impacts to the heritage resource are expected through relocation. In addition, relocation of the structure as whole would be impossible due to its construction type.	Yes – alterations to the resource are expected through relocation.	No impact.	Yes – relocation of the resource will isolate it from its original context and relationship to the East Holland River.	No impact.	Yes – the adaptive re-use of the structure for purposes other than pedestrian purposes would result in a change from the original use of the structure.	Yes – impacts are expected through process of removing the structure from its current location.
8) Replacement/removal of existing structure with salvage elements/members of heritage bridge for incorporation into new structure or for future conservation work or displays	Yes - impacts to the cultural heritage values of the East Holland Barrier culvert are expected through the complete removal of the bridge.	Yes – alterations to the resource are expected through removal.	No impact.	No impact.	Yes- views to and from the structure are expected to be altered.	No impact.	Yes – impacts are expected through removal of the existing structure and the introduction of a new bridge.
9) Replacement/removal of existing structure with full recording and documentation of the heritage bridge	Yes - impacts to the cultural heritage values of the structure are expected through the complete removal of the bridge.	Yes – alterations to the resource are expected through removal, which result in significant negative impacts to its contextual value.	No impact.	No impact.	Yes- views to and from the structure are expected to be altered.	No impact.	Yes – impacts are expected through removal of the existing structure and the introduction of a new bridge.



## **8.0 CONCLUSIONS**

Based on the results of archival research, an analysis of culvert design and construction in Ontario, field investigations, and application of Regulation 9/06 of the *Ontario Heritage Act*, the East Holland Barrier culvert was determined to possess heritage value. The subject culvert was constructed in the early twentieth century using “flat-top” design, directions for which were provided by the MTO in the 1902 Annual Report.

## **9.0 RECOMMENDATIONS**

Based on the results of archival research, an analysis of culvert design and construction in Ontario, field investigations, and heritage evaluation, the East Holland Barrier culvert was determined to retain cultural heritage value following application of Regulation 9/06 of the *Ontario Heritage Act*, and therefore should be considered for municipal designation.

Given the identified cultural heritage value of the East Holland River Barrier culvert, the following recommendations and mitigation measures should be considered and implemented:

1. Conservation Alternatives 1 -3 are the preferred conservation options, with Alternative 1, the retention of the structure with no major modifications undertaken, being the most preferred.
2. Should retention or relocation of the structure be chosen as the preferred conservation option (one of Conservation Alternatives 1 – 7), the heritage attributes identified in Section 5.1 should be retained and treated sympathetically.
3. Should replacement of the structure be chosen, (Conservation Alternatives 8 or 9), the following mitigation options should be considered:
  - c. Replacement/removal of the existing structure and construction of a new bridge. Where possible, salvage elements of the structure for incorporation into the new structure or for future conservation work or displays. The heritage attributes identified in Section 5.1 should be considered for replication.
  - d. Compatible new development, where a new bridge is given a design that is sympathetic to the design qualities of the original bridge and its setting. This option would allow simplification of original design details and the use of new technologies and materials.
4. Should replacement of the structure be chosen, a full documentation report of the structure is required. A documentation report should be completed even if a new structure is designed to replicate the existing structure sympathetically.
5. This report should be submitted to heritage staff at the Town of Aurora, the Ministry of Transportation, and the Ministry of Tourism, Culture, and Sport for review and comment.



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<http://precast.org/2014/01/precast-concrete-culvert-more-than-meets-eye/>



**APPENDIX A: Photographic Plates**



Plate 1: Looking north toward the subject culvert.



Plate 2: Looking south toward the subject culvert.



Plate 3: Oblique view of the south side of the subject culvert, looking east.



Plate 4: Oblique view of the south side of the subject culvert, looking west.



Plate 5: Oblique view of the north side of the culvert, looking west



Plate 6: Oblique view of the north side of the culvert, looking east.



Plate 7 View along the pedestrian trail toward the subject culvert.

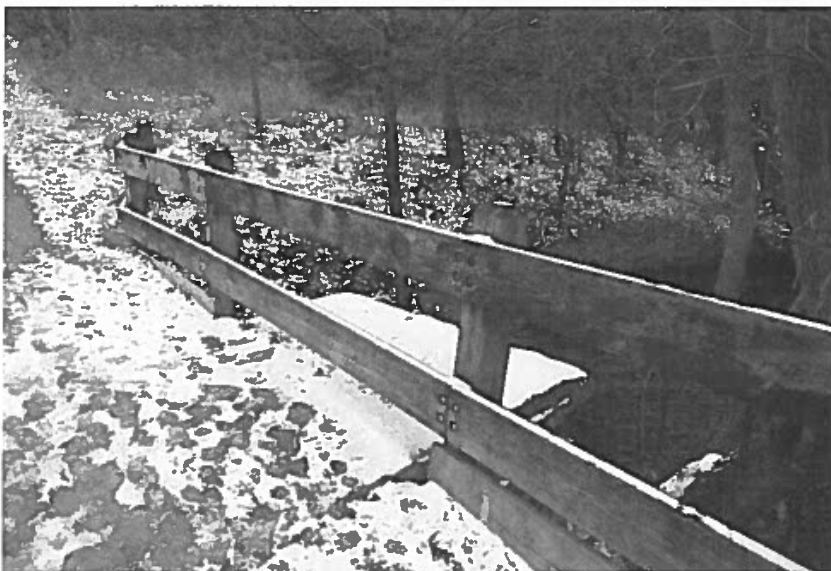


Plate 8: Detail of the wood railing system atop the subject culvert.



Plate 9: Detail of the concrete top of the subject culvert.



Plate 10: Detail of the concrete wall of the subject culvert.





Plate 11: Detail of the watercourse running through the subject culvert.

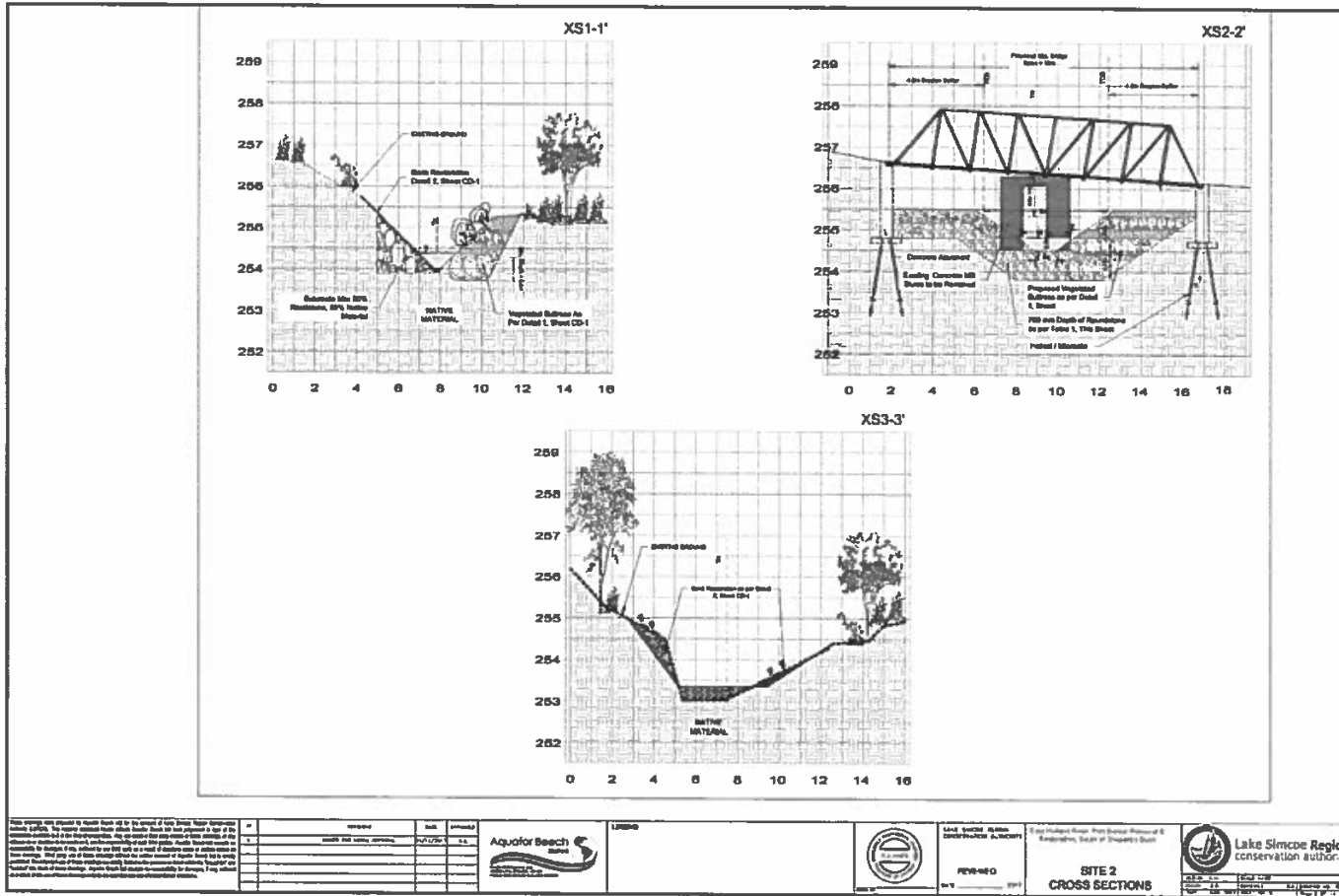


Plate 12: View toward the west wall of the subject culvert. Note the line pattern left by the formwork.

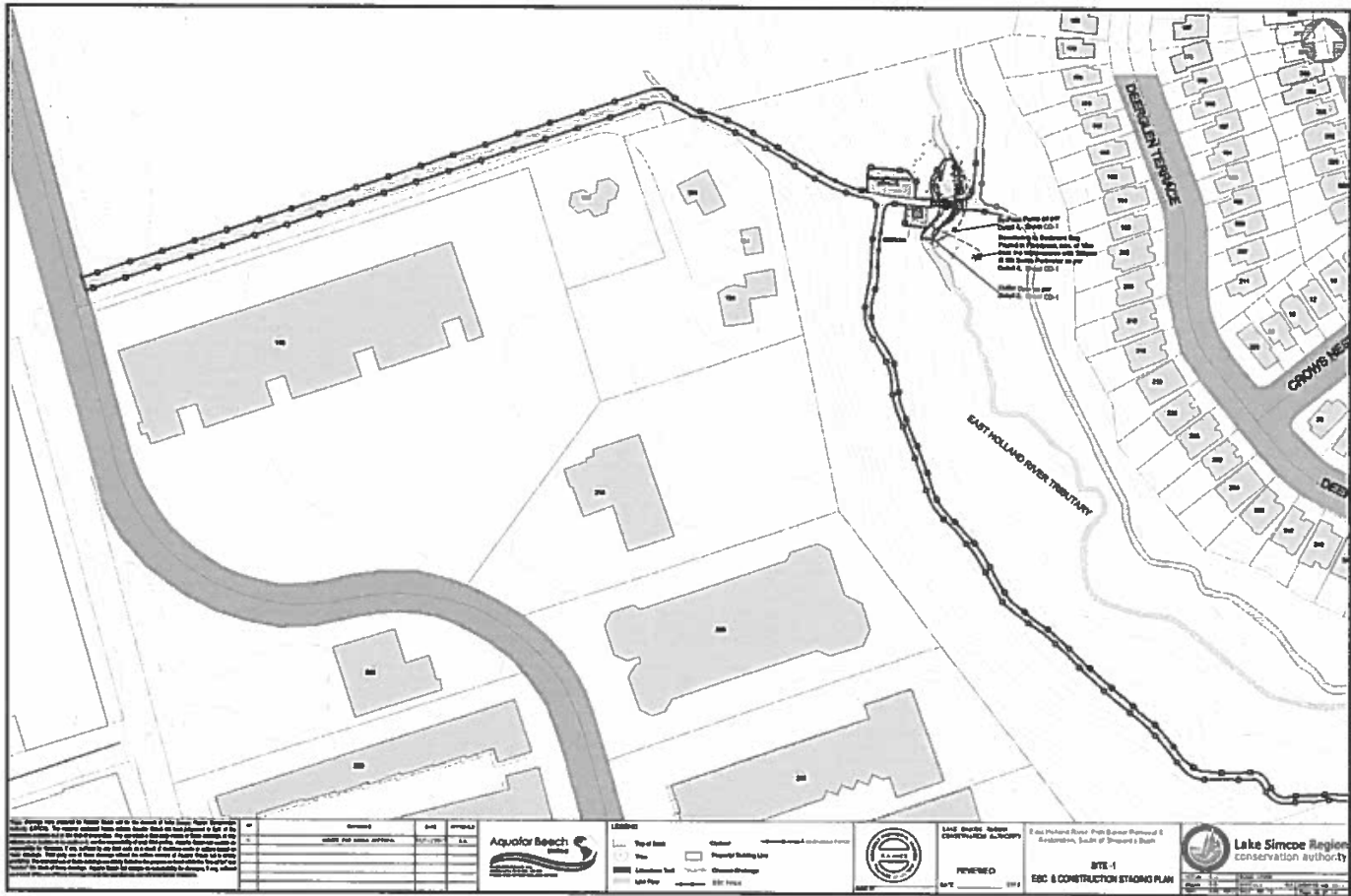
**APPENDIX B: Select Design Drawings of the Preferred Alternative**

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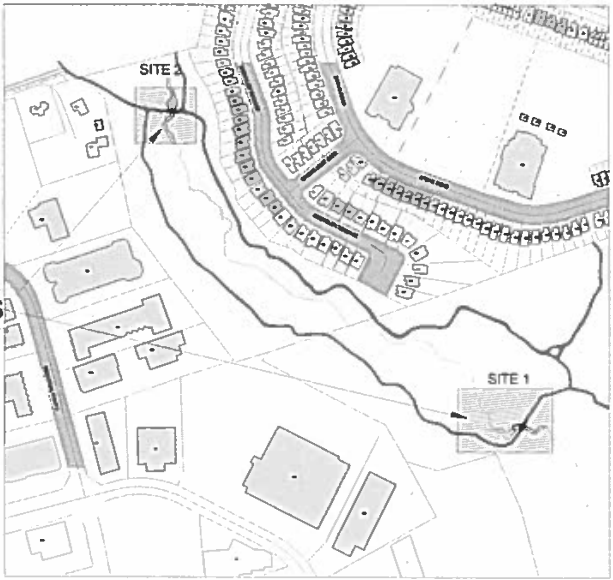


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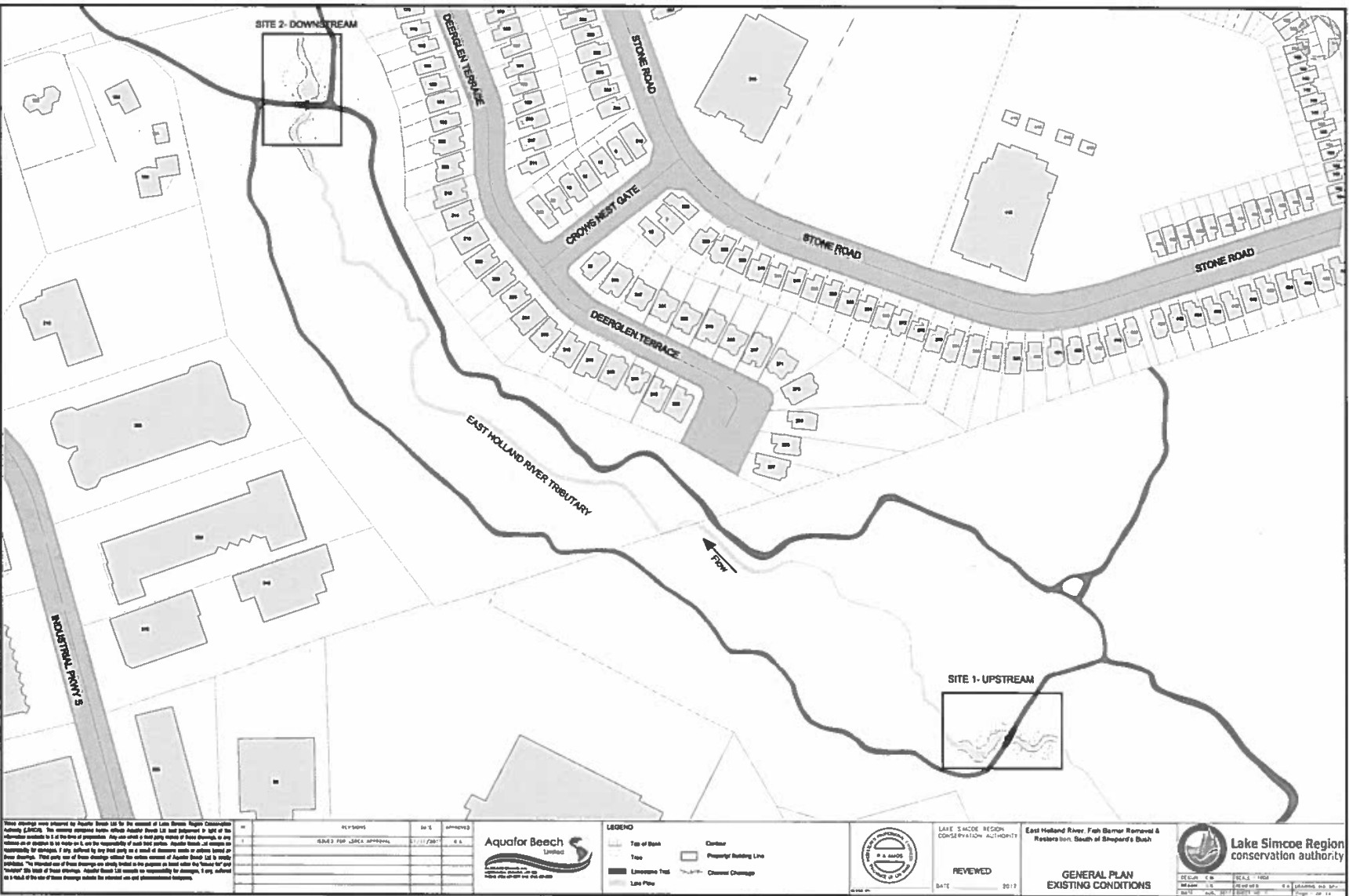
# Lake Simcoe Region conservation authority

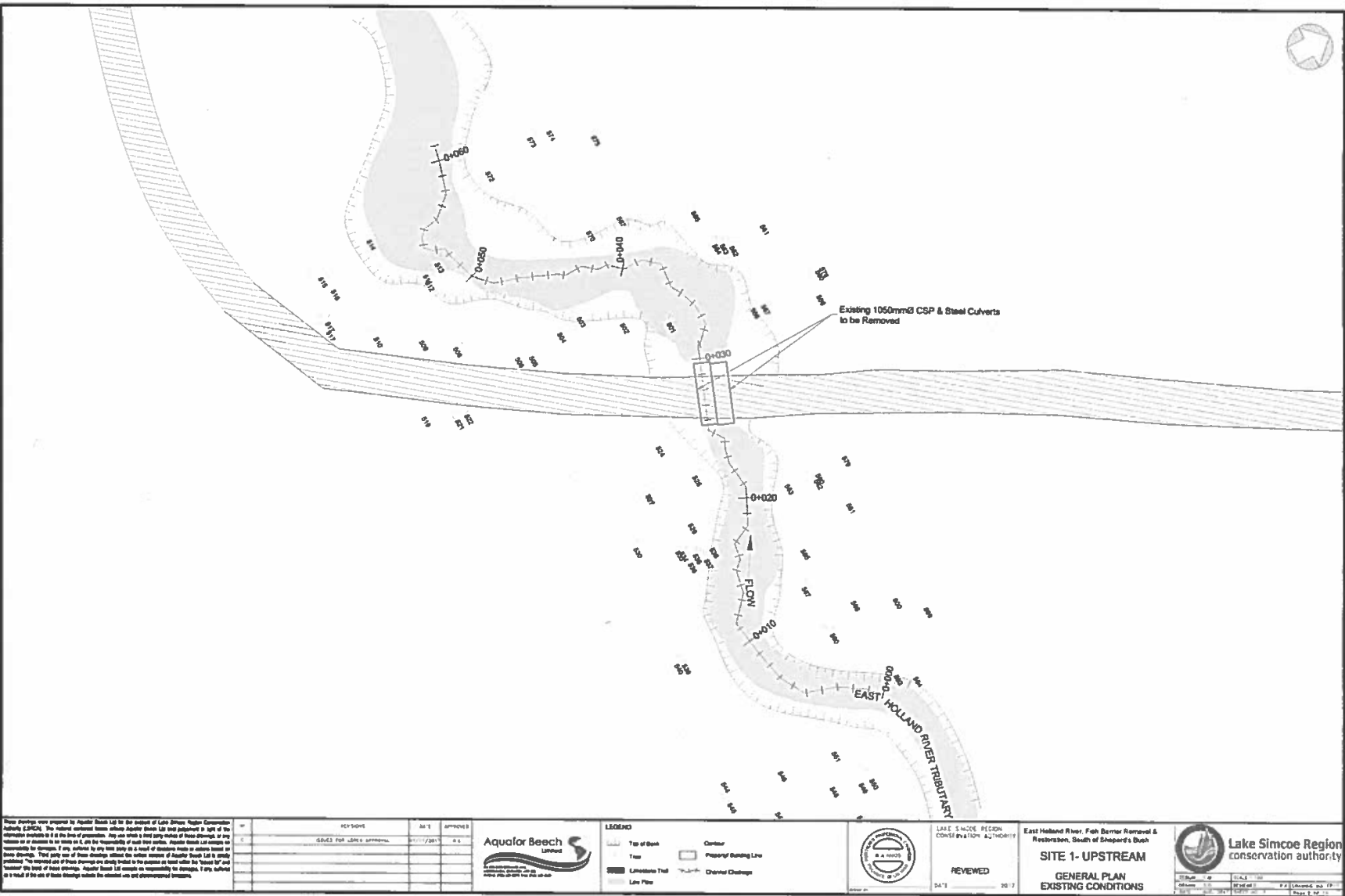
Consulting Services for East Holland River, Fish Barrier Removal & Restoration, South of Shepard's Bush



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PLAN AND PROFILE (SITE 1)	PP-1
SECTIONS (SITE 1)	XS-1
EXISTING PLAN (SITE 2)	EP-2
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NO.	REVISIONS	DATE	APPROVED
1	ISSUED FOR OWNER APPROVAL	2017/12/20/18	A.S.



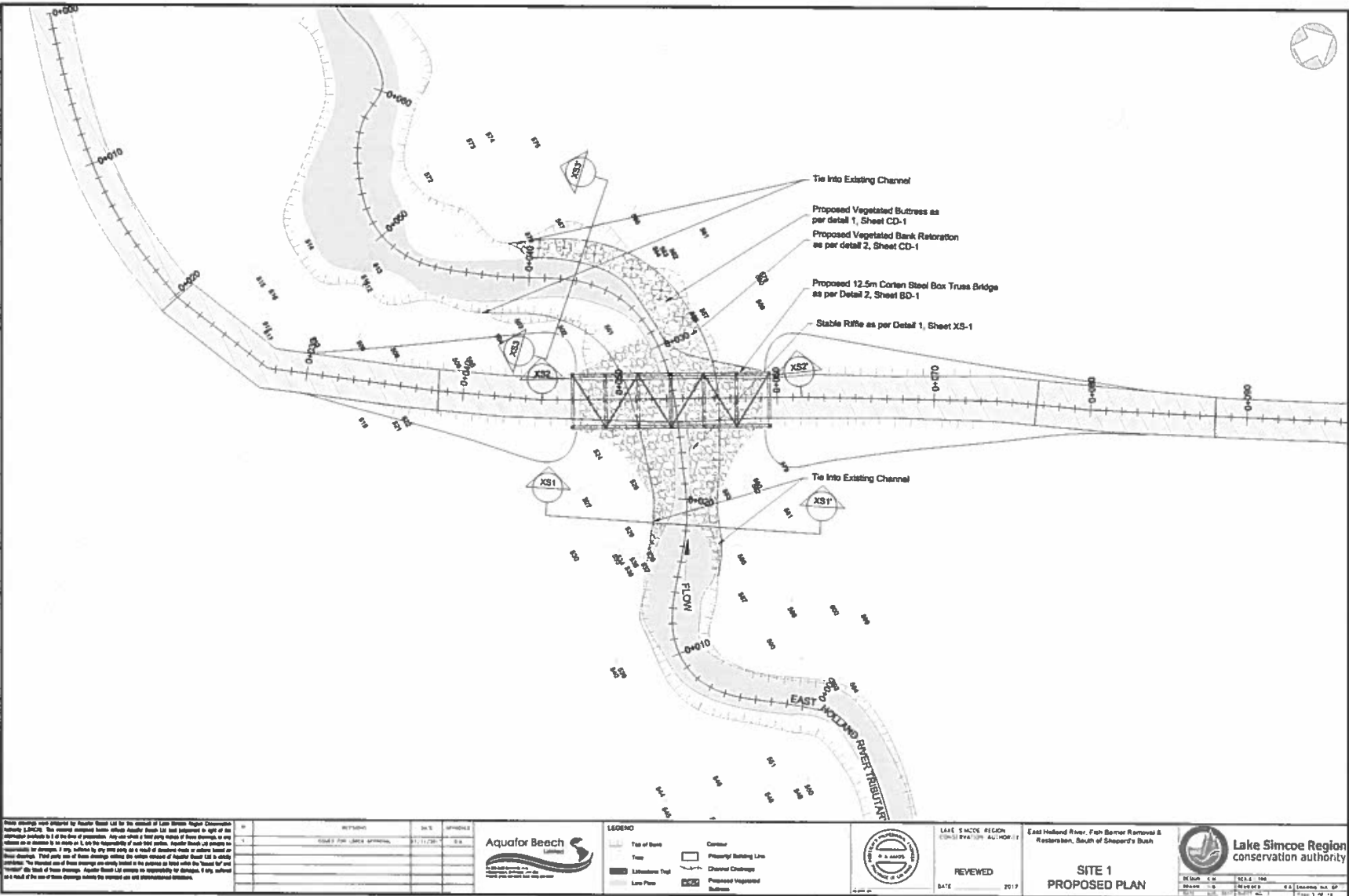
LEGEND	
	Top of Bank
	True
	Limitation Tool
	Low Flow
	Centre
	Proposed Damming Line
	Channel Change

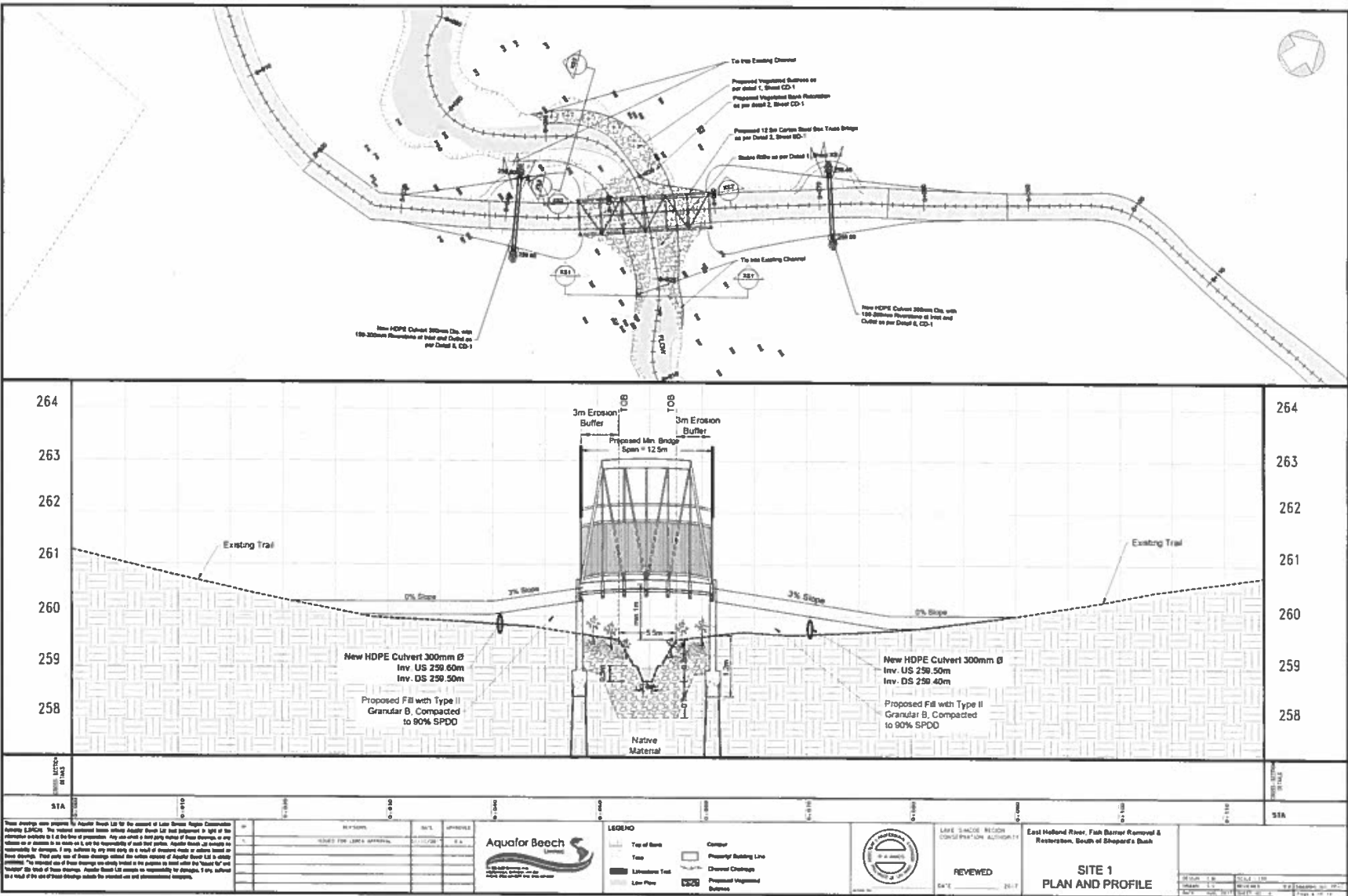


East Holland River: Fish Barrier Removal & Restoration, South of Sheppard's Bush  
**SITE 1- UPSTREAM**  
 GENERAL PLAN  
 EXISTING CONDITIONS

REVIEWED  
 2017







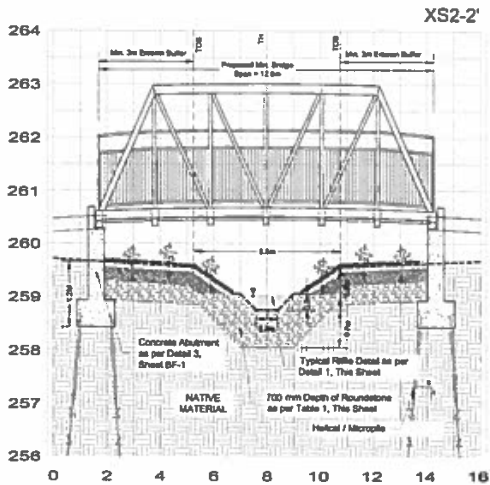
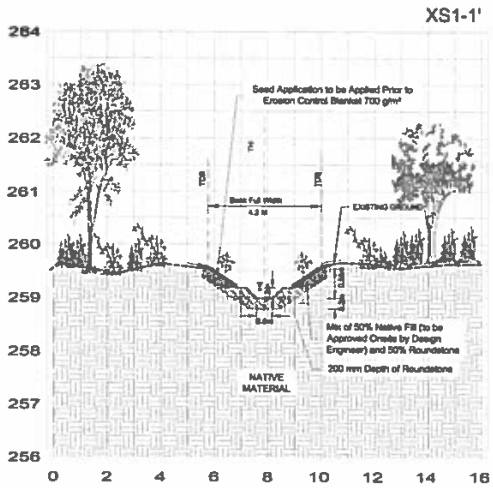
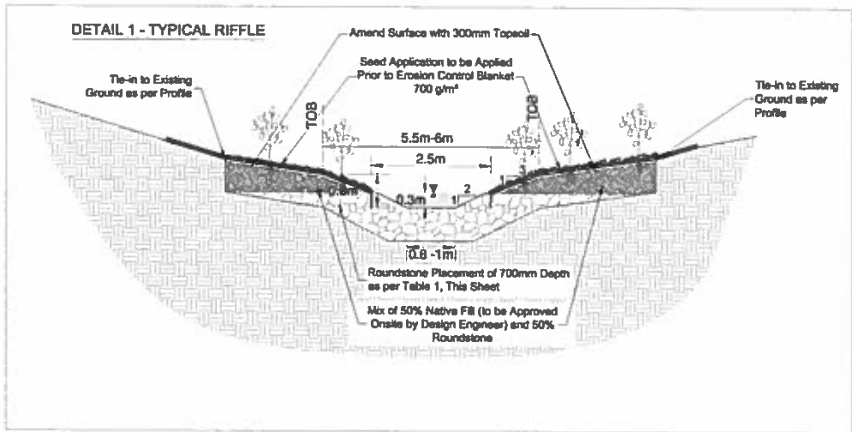
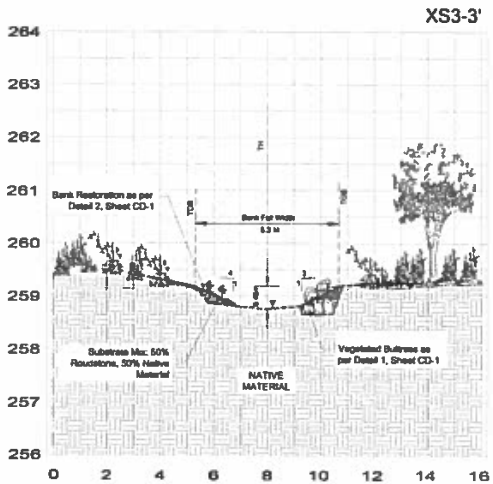


TABLE 1: ROUNDSTONE GRADATION

Stone Sizing		Percent Composition
(mm)	(%)	
0 - 10.75 (D <sub>15</sub> )	0 - 0.5 (Granular B)	30
10.75 - 75 (D <sub>30</sub> )	0.5 - 3	25
75 - 250	3 - 10	35
250 - 450 (D <sub>60</sub> )	10 - 18	30
450 - 600	18 - 24	10
Exystones (min 760)	min 30	-

NOTES:  
 1. CONTRACTOR TO INSURE THAT KEYSTONE'S ARE SIZED ACCORDING TO SPECIFICATIONS AND THAT FULL GRADATION IS INCORPORATED INTO STRUCTURE.  
 2. ACCURACY OF STONE SIZE GRADATION TO BE INSPECTED BY QUALIFIED ENGINEER PRIOR TO PLACEMENT.



These drawings were prepared by Aquaford Beech Ltd. for the account of Lake Simcoe Region Conservation Authority (LSRCA). The engineer certifies that these drawings were prepared in accordance with the provisions of the Professional Engineers Act, R.S.O. 1990, Chapter 192, and the Professional Engineers Regulation, R.R.O. 1997, Chapter 192/01. The engineer is not responsible for any errors or omissions in these drawings or for any consequences that may result from the use of these drawings. The user of these drawings shall be responsible for any errors or omissions in these drawings or for any consequences that may result from the use of these drawings. Aquaford Beech Ltd. is not responsible for any errors or omissions in these drawings or for any consequences that may result from the use of these drawings. Aquaford Beech Ltd. is not responsible for any errors or omissions in these drawings or for any consequences that may result from the use of these drawings.

NO.	REVISIONS	DATE	APPROVALS
1	ISSUED FOR LOCAL APPROVAL	01/17/20	E.A.

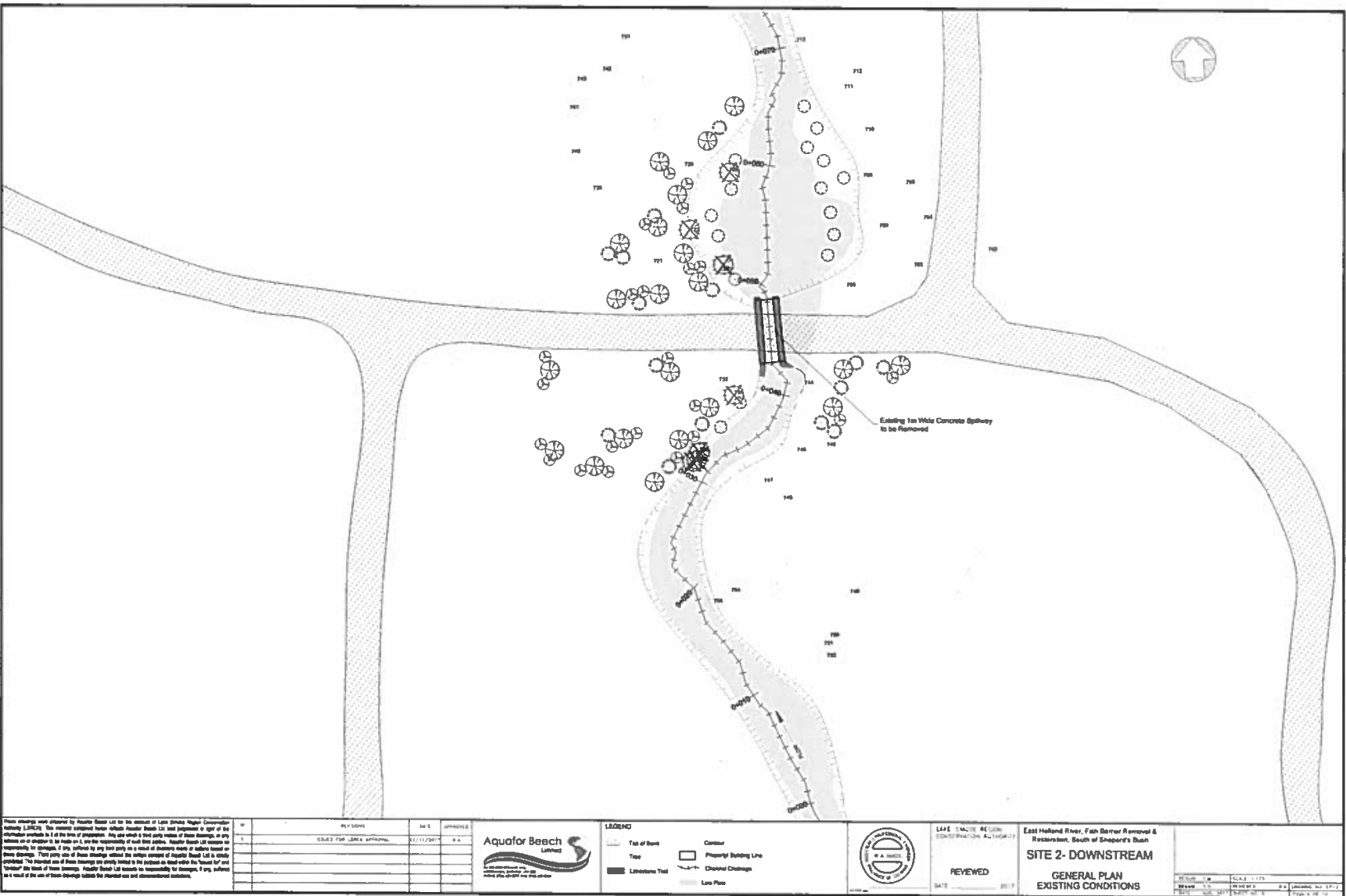


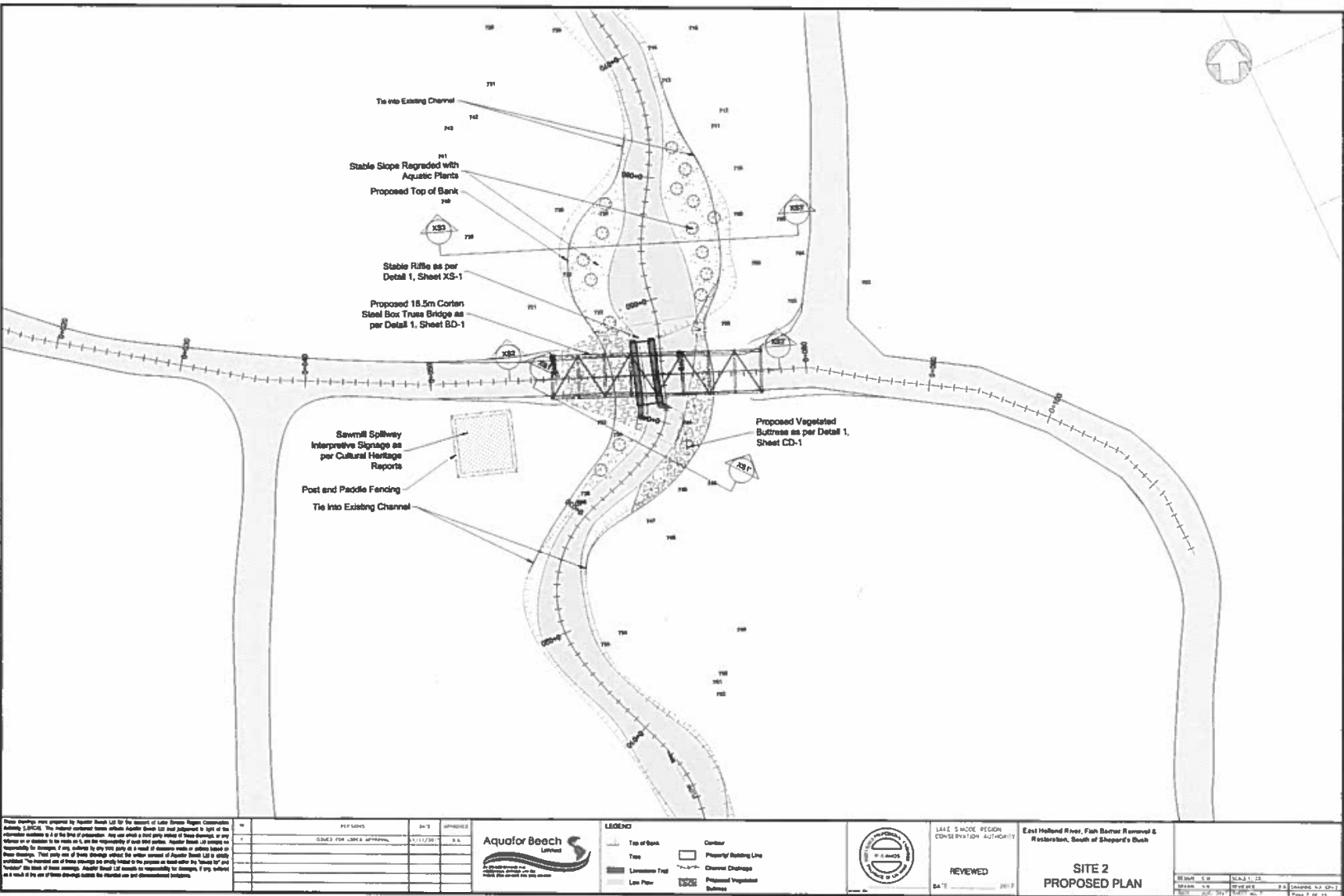
LSRCA  
 REVIEWED  
 DATE 2017



LAKE SIMCOE REGION  
 CONSERVATION AUTHORITY  
 East Holland River, Fish Barrier Removal & Restoration, South of Sheppard's Bush  
 SITE 1  
 CROSS SECTIONS







These drawings were prepared by Aquafor Beach Ltd for the benefit of Lake Simcoe Region Conservation Authority (LSRCA). The engineer certifies herein without Aquafor Beach Ltd and judgement to light of the information contained in it or the kind of information. Any use which is not intended or intended for any other use or in connection to the main use is not the responsibility of Aquafor Beach Ltd. The user assumes all responsibility for the drawings. If any liability for any third party as a result of damages made or actions taken on these drawings. That party will be deemed to have accepted the entire content of Aquafor Beach Ltd's drawings. The user will be deemed to have accepted the entire content of Aquafor Beach Ltd's drawings. The user will be deemed to have accepted the entire content of Aquafor Beach Ltd's drawings. The user will be deemed to have accepted the entire content of Aquafor Beach Ltd's drawings.

NO.	REV.	DATE	APPROVED
1		2017/12/20	A.S.
2			
3			
4			
5			



**LEGEND**

	Top of Bank		Contour
	Riffle		Proposed Riffle Line
	Channel Change		Proposed Channel Change
	Limestone Trial		Proposed Limestone Trial
	Low Flow		Proposed Low Flow
	Proposed Vegetation		Proposed Buttress



LAKE SIMCOE REGION  
 CONSERVATION AUTHORITY

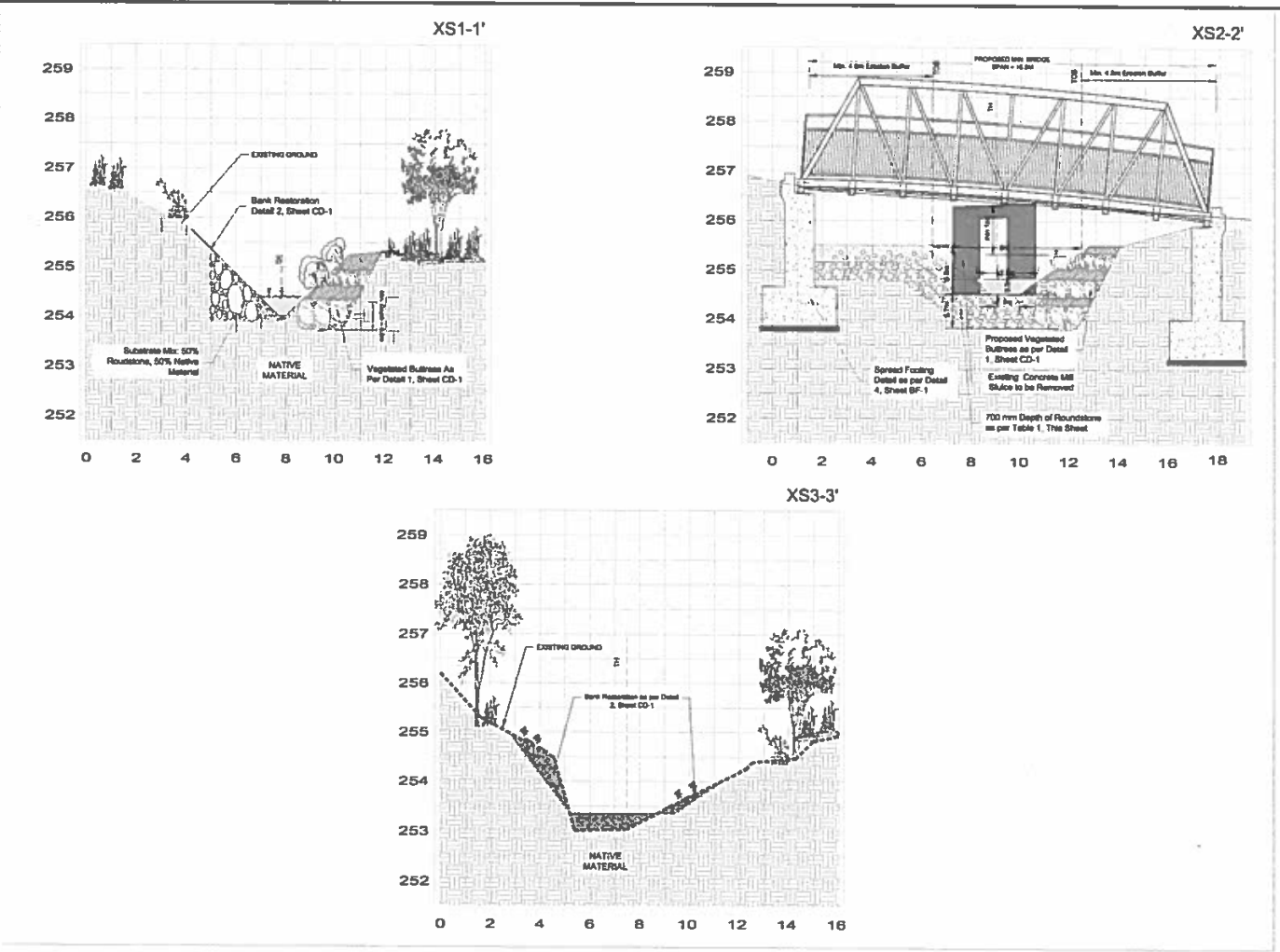
REVIEWED

DATE: 2018

**SITE 2  
 PROPOSED PLAN**

PROJECT NO.	DATE	SCALE	PROJECT NAME
18-001	2018	1:1	East Holland River, Fish Barren Removal & Restoration, South of Shepard's Bush
DESIGNED BY	CHECKED BY	DATE	PROJECT NAME
A.S.	A.S.	2018	East Holland River, Fish Barren Removal & Restoration, South of Shepard's Bush





These drawings were prepared by Aquator Beach Ltd for the account of Lake Simcoe Region Conservation Authority (LSRCA). The original contract between Aquator Beach Ltd and LSRCA is available at the LSRCA website in 1 of the links of presentation. You can check a final copy version of these drawings at the website or in relation to its terms on L, see the responsibility of each party. Aquator Beach Ltd accepts no responsibility for damages, if any, suffered by any third party as a result of reliance made or actions based on these drawings. Third party use of these drawings without the express consent of Aquator Beach Ltd is strictly prohibited. The intended use of these drawings are strictly limited to the project as stated within the "Project List" and "Intended" use terms of these drawings. Aquator Beach Ltd accepts no responsibility for damages, if any, suffered as a result of the use of these drawings outside the intended use and dimensional tolerances.

NO.	REVISED	DATE	APPROVED
1	ISSUED FOR LSRCA APPROVAL	21/11/2017	S.A.



LEGEND

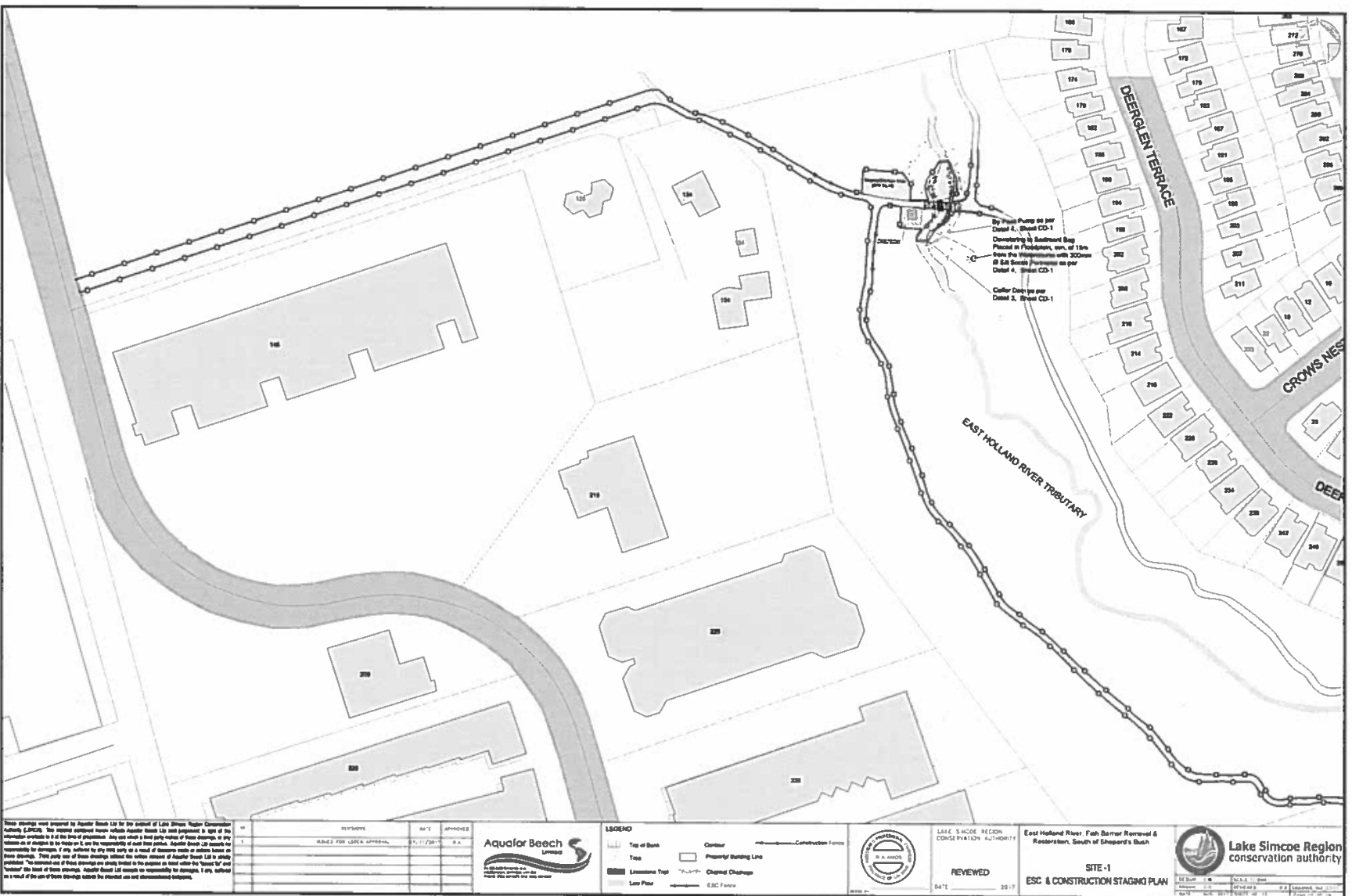



East Holland River, Fish Barrier Removal & Restoration, South of Sheppard's Bush

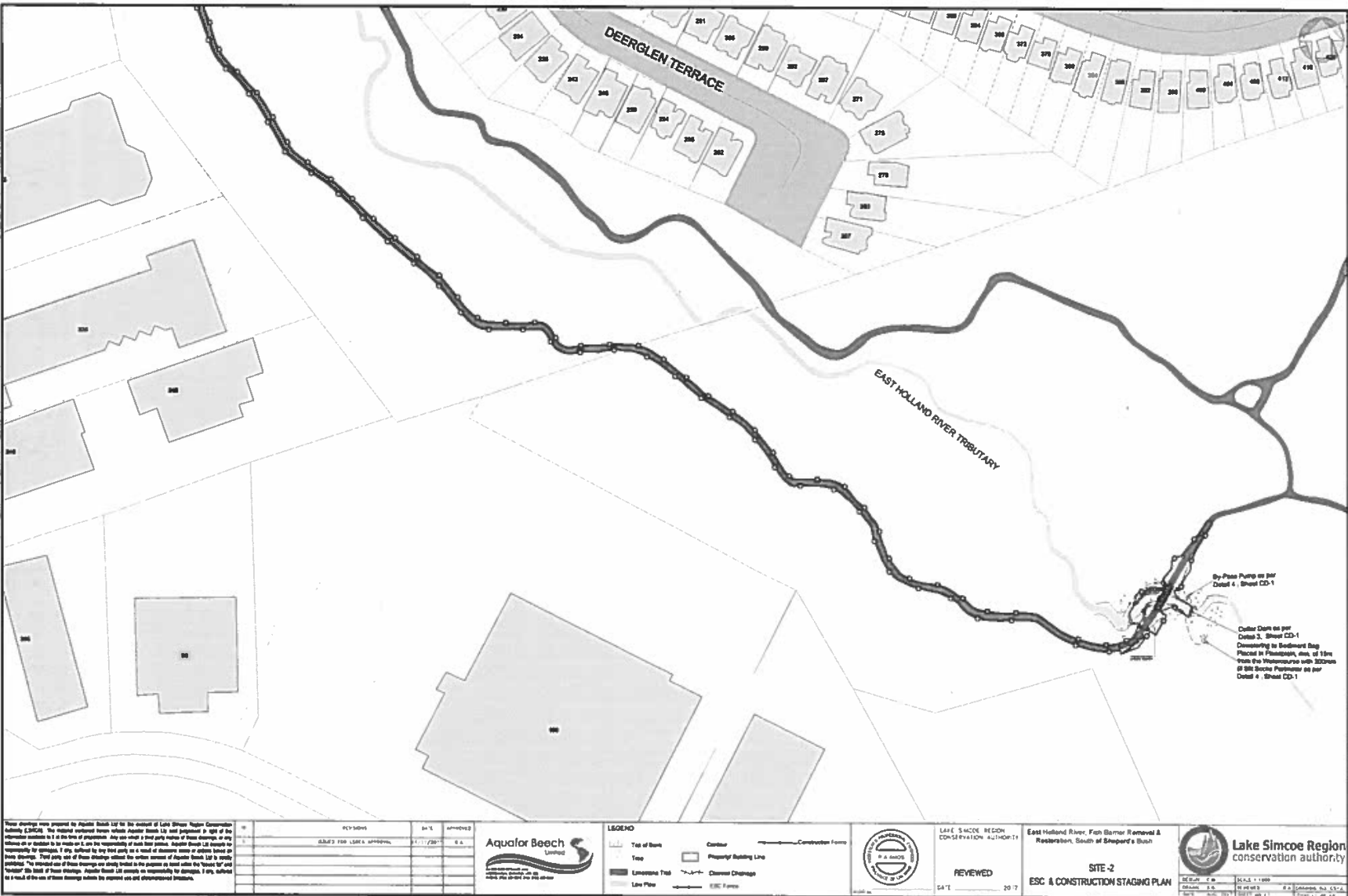
**SITE 2  
 CROSS SECTIONS**

Lake Simcoe Region  
 conservation authority

SCALE: 1:50  
 DATE: 2017  
 DRAWN BY: S.A.  
 CHECKED BY: S.A.  
 PROJECT NO.: 17-01-01  
 SHEET NO.: 5

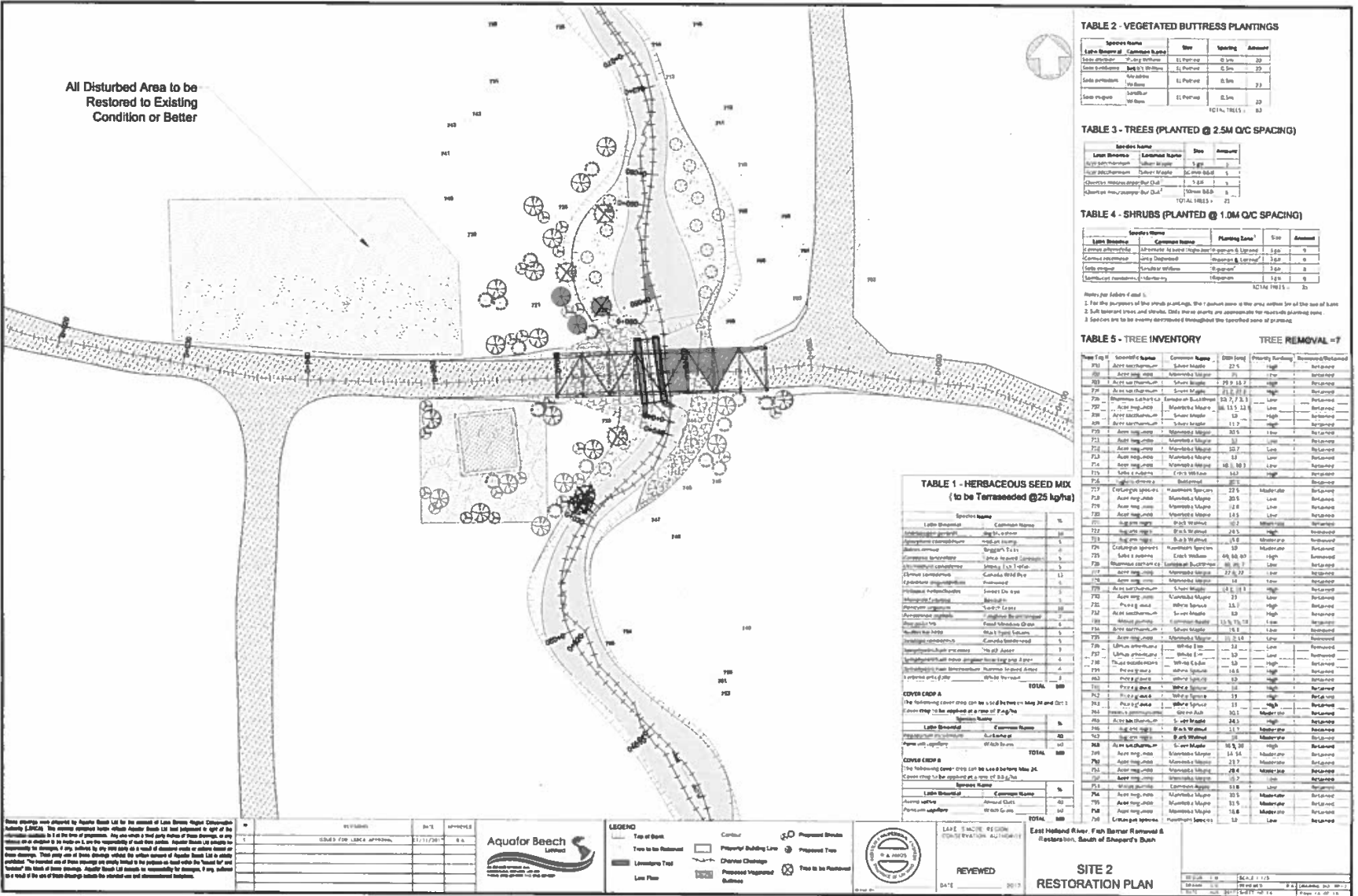




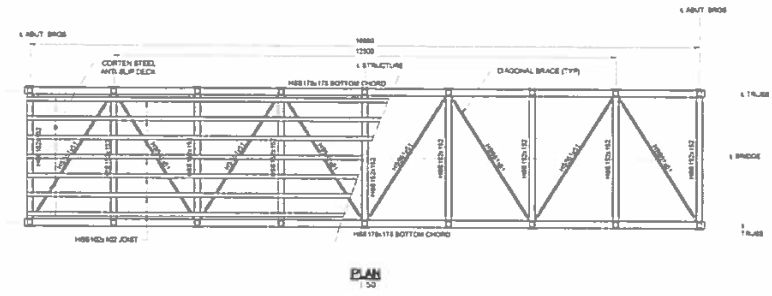




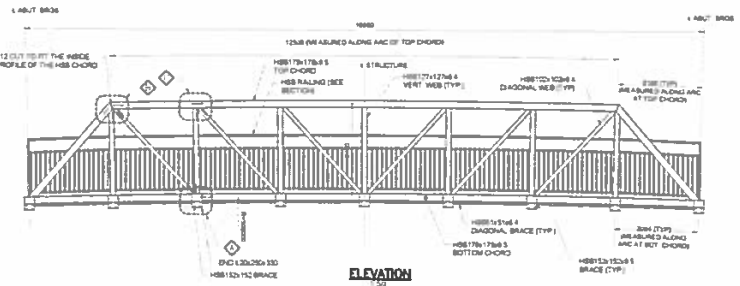




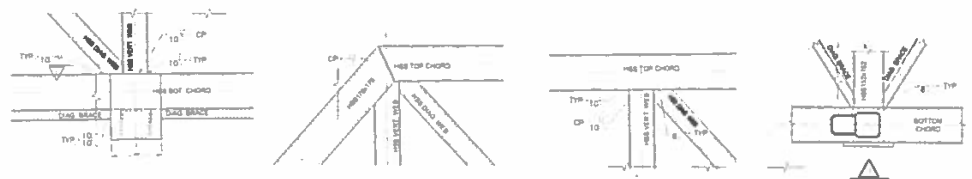
DETAIL 1: BRIDGE 1(16.5M)



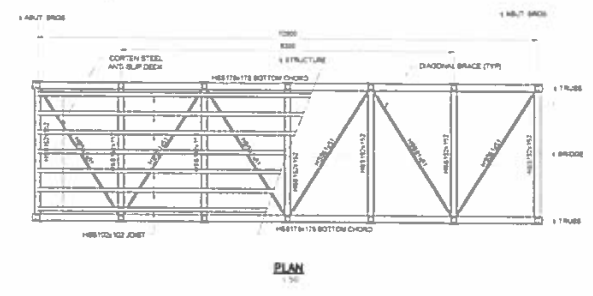
PLAN  
1:50



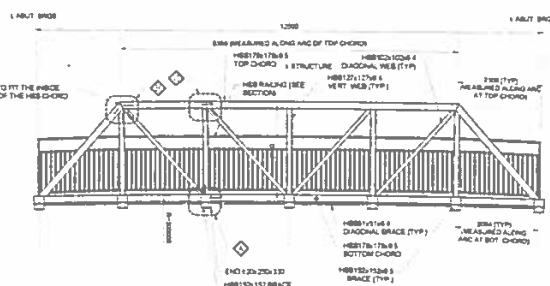
ELEVATION  
1:20



DETAIL 2: BRIDGE 2(12.5M)

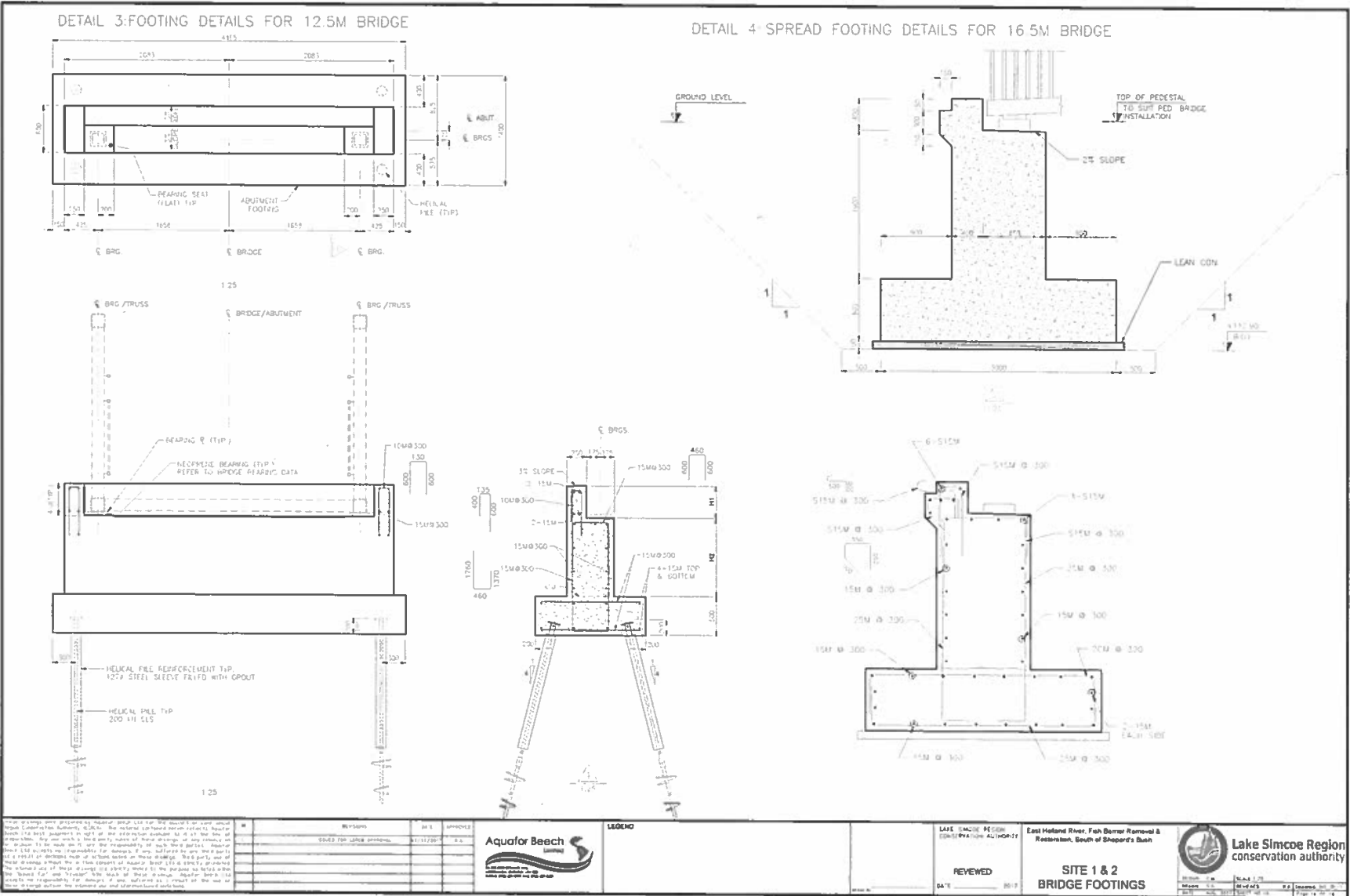


PLAN  
1:50



- NOTES**
- STRUCTURE**
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DESIGN AND FABRICATE THE BRIDGE IN CONFORMANCE WITH THE CANADIAN HIGHWAY BRIDGE DESIGN CODE (CHBDC, CANCSA-38), THE CANADIAN NATIONAL BUILDING CODE (CNBC, 2009), AND WITH ACCESSIBILITY FOR ONTARIANS WITH DISABILITIES ACT, 2005 (AODA). THE ENGINEER MAY REFER TO INTEGRATED ACCESSIBILITY STANDARDS REGULATION GUIDELINES (APRIL, 2014) FOR DESIGN RECOMMENDATIONS.
  - THE STRUCTURE IS TO BE THE TYPE AND DIMENSIONS AS SHOWN AS TYPICAL ON THE CONTRACT DRAWINGS, DESIGNED AND STAMPED BY TWO ONTARIO PROFESSIONAL ENGINEERS.
  - THE BRIDGE IS TO BE EAGLE BRIDGE OR APPROVED EQUAL, CONSISTING OF TWO TRUSSES AND LATERAL BRACING BELOW THE FLOOR SYSTEM.
  - THE DESIGNED BEARINGS/URCHARGE CAPACITY FOR THE BRIDGES SHALL BE 8 KPA AND SHALL SUPPORT THE WEIGHT OF OCCASIONAL ACCESS FOR MEDIUM TRUCKS AND LIGHT EMERGENCY VEHICLES (IE. AMBULANCES).
  - THE WALKING SURFACE SHALL BE 4 METRES WIDE, UNLESS OTHERWISE NOTE, WITH NO ENCROACHMENTS INTO THIS SPACE BY RAILING OR CURB STRUCTURES. THE FLOOR SYSTEM SHALL BE ANTI-SLIP SELF WEATHERING STEEL DECKING.
  - RAILING HEIGHTS SHALL BE A MINIMUM OF 1.37 M HIGH IN ORDER TO SATISFY CYCLING SAFETY REQUIREMENTS. HAND RAILINGS SHALL BE STAINLESS STEEL.
  - EACH STRUCTURE SHALL MEET THE DIMENSIONS SHOWN ON THE CONTRACT DRAWINGS AND NOTED IN THE ADJACENT TABLE.
- CONCRETE**
- ALL WORKS SHALL BE IN COMPLIANCE WITH OPSS 501, OPSS804, OPSS 905, OPSS 919, AND OPSS 920, WHERE REQUIRED, OPSS 923 WILL APPLY.
  - ALL CONCRETE PRODUCTION AND PLACEMENT TO CONFORM TO CSA STANDARD CAN1-A23-1-M.
  - ABUTMENTS A-30 MPa, 5-7% AIR ENTRAINMENT.
  - REINFORCING STEEL SHALL BE IN ACCORDANCE WITH CSA STANDARD C308-18, GRADE 400.

<p>THIS DRAWING AND ALL INFORMATION CONTAINED HEREIN ARE THE PROPERTY OF AQUAFOR BEACH LIMITED. NO PART OF THIS DRAWING OR INFORMATION CONTAINED HEREIN IS TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF AQUAFOR BEACH LIMITED.</p>	REVISED	DATE	BY	APPROVED		<p>LEGEND</p>	<p>LAKE SIMCOE REGION CONSERVATION AUTHORITY</p>	<p>East Holland River, Fish Barrier Removal &amp; Restoration, South of Sheppard's Bush</p>	
	REVIEWED	DATE	BY	APPROVED					



Attachment 3

## ***#29 Oakridge's Trail***

***OSIM Inspection***

***Date of Inspection: 25/07/2015***

<b>Element Group:</b>	Abutment				<b>Length:</b>	5.5m		
<b>Element Name:</b>	Abutment Walls (Legs)				<b>Width:</b>	0.55m		
<b>Location:</b>					<b>Height:</b>	1.5m		
<b>Material:</b>	Concrete				<b>Count:</b>	2		
<b>Element Type:</b>					<b>Total Quantity:</b>	16.5 sq. m		
<b>Environment:</b>	Moderate				<b>Limited Inspection</b>			
<b>Protection System:</b>					<b>Perform. Def.</b>		<b>Maint. Needs</b>	
<b>Condition</b>	<b>Units</b>	<b>Exc.</b>	<b>Good</b>	<b>Fair</b>	<b>Poor*</b>			
<b>Data:</b>	m <sup>2</sup> / m / each / % / all		40%	30%	30%			
<b>Comments:</b> Wide cracks on South East Interior side and North East Exterior side. Medium scaling throughout. Very severe spalling on the exterior faces.								
<b>Recommended Work:</b> None <input type="checkbox"/> 6 - 10 years <input type="checkbox"/> 1 - 5 years <input type="checkbox"/> < 1 year <input checked="" type="checkbox"/> Urgent <input type="checkbox"/>								





<b>Element Group:</b>	Abutment		<b>Length:</b>	4.55m (West) 3.2m (East)			
<b>Element Name:</b>	Wing walls		<b>Width:</b>				
<b>Location:</b>			<b>Height:</b>	2m			
<b>Material:</b>	Concrete		<b>Count:</b>				
<b>Element Type:</b>			<b>Total Quantity:</b>	4.55sq. m (West) 3.2sq. m (East)			
<b>Environment:</b>	Moderate		<b>Limited Inspection</b>				
<b>Protection System:</b>						<b>Perform. Def.</b>	<b>Maint. Needs</b>
<b>Condition Data:</b>	<b>Units</b> m <sup>2</sup> / m / each / % / all	<b>Exc.</b>	<b>Good</b> 10%	<b>Fair</b> 45%	<b>Poor*</b> 45%		
<b>Comments:</b> Two wing walls on the North side are destroyed and laying on the ground and stream. Two standing both have wide cracking.							
<b>Recommended Work:</b> None <input type="checkbox"/> 6 - 10 years <input type="checkbox"/> 1 - 5 years <input type="checkbox"/> < 1 year <input checked="" type="checkbox"/> Urgent <input type="checkbox"/>							



<b>Element Group:</b>	Deck				<b>Length:</b>		
<b>Element Name:</b>	Deck Top				<b>Width:</b>		
<b>Location:</b>					<b>Height:</b>		
<b>Material:</b>	Gravel Surface				<b>Count:</b>		
<b>Element Type:</b>					<b>Total Quantity:</b>		
<b>Environment:</b>	Moderate				<b>Limited Inspection</b>		
<b>Protection System:</b>						<b>Perform. Def.</b>	<b>Maint. Needs</b>
<b>Condition</b>	<b>Units</b>	<b>Exc.</b>	<b>Good</b>	<b>Fair</b>	<b>Poor*</b>		
<b>Data:</b>	m <sup>2</sup> / m / each / % / all		90%	10%			
<b>Comments:</b> Walking path is in fair to good condition.							
<b>Recommended Work:</b> None <input checked="" type="checkbox"/> 6 - 10 years <input type="checkbox"/> 1 - 5 years <input type="checkbox"/> < 1 year <input type="checkbox"/> Urgent <input type="checkbox"/>							



<b>Element Group:</b>	Embankments and Stream				<b>Length:</b>	
<b>Element Name:</b>	Streams and Waterways				<b>Width:</b>	
<b>Location:</b>					<b>Height:</b>	
<b>Material:</b>					<b>Count:</b>	2
<b>Element Type:</b>					<b>Total Quantity:</b>	2
<b>Environment:</b>					<b>Limited Inspection</b>	
<b>Protection System:</b>						<b>Perform. Def.</b>
<b>Condition</b>	<b>Units</b>	<b>Exc.</b>	<b>Good</b>	<b>Fair</b>	<b>Poor*</b>	
<b>Data:</b>	m <sup>2</sup> / m / each / % / all		100%			02
<b>Comments:</b> Generally in good condition.						
<b>Recommended Work:</b> None <input checked="" type="checkbox"/> 6 - 10 years <input type="checkbox"/> 1 - 5 years <input type="checkbox"/> < 1 year <input type="checkbox"/> Urgent <input type="checkbox"/>						



<b>Element Group:</b>	Deck				<b>Length:</b>	5.5m	
<b>Element Name:</b>	Thin Slab Soffit				<b>Width:</b>	2.1m	
<b>Location:</b>					<b>Height:</b>	0.25m	
<b>Material:</b>	Concrete				<b>Count:</b>	1	
<b>Element Type:</b>					<b>Total Quantity:</b>	6.55m	
<b>Environment:</b>	Moderate				<b>Limited Inspection</b>		
<b>Protection System:</b>					<b>Perform. Def.</b>		<b>Maint. Needs</b>
<b>Condition</b>	<b>Units</b>		<b>Exc.</b>	<b>Good</b>	<b>Fair</b>	<b>Poor*</b>	
<b>Data:</b>	m <sup>2</sup> / m / each / % / all			35%	35%	30%	
<b>Comments:</b> Severe delamination. Tree growth on and through the soffit.							
<b>Recommended Work:</b> None <input type="checkbox"/> 6 - 10 years <input type="checkbox"/> 1 - 5 years <input type="checkbox"/> < 1 year <input checked="" type="checkbox"/> Urgent <input type="checkbox"/>							



<b>Element Group:</b>	Barriers				<b>Length:</b>	8m		
<b>Element Name:</b>	Fence				<b>Width:</b>			
<b>Location:</b>					<b>Height:</b>			
<b>Material:</b>	Wood				<b>Count:</b>	2		
<b>Element Type:</b>					<b>Total Quantity:</b>	16m		
<b>Environment:</b>	Moderate				<b>Limited Inspection</b>			
<b>Protection System:</b>						<b>Perform. Def.</b>		<b>Maint. Needs</b>
<b>Condition Data:</b>	<b>Units</b>		<b>Exc.</b>	<b>Good</b>	<b>Fair</b>	<b>Poor*</b>		
	m2 / m / each / % / all			90%	10%			
<b>Comments:</b> Fair to good condition.								
<b>Recommended Work:</b> None <input checked="" type="checkbox"/> 6 - 10 years <input type="checkbox"/> 1 - 5 years <input type="checkbox"/> < 1 year <input type="checkbox"/> Urgent <input type="checkbox"/>								





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Aurora, Ontario  
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**Phone:** 905-727-3123 ext. 4349  
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**www.aurora.ca**

**Town of Aurora  
Planning and Development  
Services**

## **Memorandum**

**Date:** March 5, 2018  
**To:** Heritage Advisory Committee  
**From:** Jeff Healey, Planner/Heritage Planning  
**Copy:** Marco Ramunno, Director of Planning and Development Services  
**Re:** **Building Tension: When to tear down and when to build up**

---

### **Recommendation**

- 1. That the memorandum regarding Building Tension: When to tear down and when to build up, be received for information.**

### **Background**

The link provided below is a CBC Radio Ideas podcast on heritage conservation in Canada. The podcast comprises a panel of heritage conservation architects providing their ideas on the changing face of Canadian cities, while using the example of the City of Halifax for local examples and context. The panel provides their ideas on scale, heritage and modernity, and offers solutions for municipalities, developers and community advocates.

Link to podcast: <http://www.cbc.ca/radio/ideas/building-tension-when-to-tear-down-and-when-to-build-up-1.4165001>

### **Attachments**

None



**Extract from  
Council Meeting of  
Tuesday, February 13, 2018**

**6. Consideration of Items Requiring Discussion (Regular Agenda)**

**R3. Summary of Committee Recommendations Report No. 2018-01  
(Formerly Item C1(R5))**

**Moved by Councillor Gaertner  
Seconded by Councillor Thompson**

1. That Summary of Committee Recommendations Report No. 2018-01 be received; and
2. That the Committee recommendations contained within this report be approved.

**Carried**

**Recommendations from Heritage Advisory Committee Meeting Minutes of  
November 13, 2017:**

- 1. HAC17-024 – Request to Remove a Property from the Aurora Register of  
Properties of Cultural Heritage Value or Interest  
14288 Yonge Street**
  - (a) That the property located at 14288 Yonge Street be removed from the Aurora Register of Properties of Cultural Heritage Value or Interest; and
  - (b) That a financial contribution to the Heritage Reserve Fund be provided to the Town; and
  - (c) That the Owner's heritage consultant submit a photographic report of a controlled demolition of the Cannon Farmhouse to determine the building's construction date for education purposes; and
  - (d) That the Cannon Farmhouse name be commemorated by street naming and a plaque within the private condominium road and, where appropriate, along the public trail; and
  - (e) That materials salvaged from the demolished home be used in the commemorative marker/plaque.

**2. HAC17-025 – Request to Remove a Property from the Aurora Register of Properties of Cultural Heritage Value or Interest  
86 Temperance Street**

- (a) That the property located at 86 Temperance Street be removed from the Aurora Register of Properties of Cultural Heritage Value or Interest upon issuance of a demolition permit for the property; and
- (b) That future building elevations are subject to approval of Planning staff to ensure the proposed new dwelling will maintain the heritage character of the area; and
- (c) That the Black Walnut tree located in the southwest corner of the lot be preserved; and
- (d) That a future Zoning By-law amendment to change the zoning from RA2 to R3 to permit a new, single detached residential dwelling on the subject lands, in keeping with the neighbourhood, be supported by the Heritage Advisory Committee.